

*January, 1995*

*FINAL*

*Prado de Las Posas*

*Specific Plan*

(original w/original exhibits)



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***PRADO DE LAS POSAS  
SPECIFIC PLAN***

***FINAL***

***APPROVED JANUARY 11, 1995***

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***Prepared by  
CITY OF CAMARILLO  
DEPARTMENT OF PLANNING  
AND COMMUNITY DEVELOPMENT***



## PRADO DE LAS POSAS SPECIFIC PLAN

<u>Section</u>	<u>Page</u>
I. SUMMARY	
1.0 Foreword . . . . .	1
2.0 The Prado de Las Posas Plan . . . . .	3
3.0 Intent and Structure . . . . .	6
II. INTRODUCTION	
1.0 Project Location and Description . . . . .	8
2.0 Existing Conditions . . . . .	14
3.0 Background and Purpose . . . . .	23
4.0 CEQA Compliance and Authority . . . . .	25
III. DEVELOPMENT PLAN	
1.0 Opportunities and Constraints . . . . .	26
2.0 Land Use . . . . .	30
3.0 Circulation . . . . .	48
4.0 Outdoor Recreation . . . . .	57
5.0 Drainage . . . . .	58
6.0 Water . . . . .	60
7.0 Sewer . . . . .	63
8.0 Utilities . . . . .	66
IV. DEVELOPMENT REGULATIONS	
1.0 Introduction . . . . .	69
2.0 Building Height Limitations . . . . .	71
3.0 Permitted Building Coverage . . . . .	72
4.0 Minimum Parcel Size . . . . .	73
5.0 Building Setbacks . . . . .	75
6.0 Parking Requirements . . . . .	76
V. DESIGN GUIDELINES	
1.0 General Guidelines . . . . .	78
2.0 Building Design . . . . .	81
3.0 Site Planning . . . . .	92
4.0 Signage . . . . .	109
5.0 Landscape . . . . .	115
VI. IMPLEMENTATION	
1.0 Introduction . . . . .	135
2.0 Processing, Procedures and Phasing . . . . .	136
3.0 Comprehensive Maintenance . . . . .	141
4.0 Camarillo General Plan Consistency . . . . .	142

## PRADO DE LAS POSAS SPECIFIC PLAN

## LIST OF EXHIBITS &amp; TABLES

<u>Figure</u>	<u>Page</u>
1. Regional Location Map . . . . .	4
2. Vicinity Map . . . . .	5
3. Existing Land Uses . . . . .	10
4. General Plan and Zoning Designations . . . . .	11
5. Opportunities and Constraints . . . . .	29
6. Land Use Plan . . . . .	31
7. Towers . . . . .	42
8. Circulation Plan . . . . .	49
9. Proposed Storm Drain Plan . . . . .	59
10. Water Plan/Sewer Plan . . . . .	61
11. Existing Dry Utilities . . . . .	65
12. Proposed Dry Utilities . . . . .	68
13. Height Restrictions . . . . .	70
14. Mediterranean Architecture . . . . .	79
15. Mediterranean Architecture . . . . .	80
16. Windows . . . . .	85
17. Windows and Doors . . . . .	86
18. Columns . . . . .	87
19. Typical Village Commercial Facades . . . . .	88
20. Plaza Seating Area . . . . .	89
21. Planning Area I, Site Planning Concept - Commercial . . . . .	94
22. Planning Area II, Site Planning Concept - Commercial . . . . .	95
23. Planning Area IV, Site Planning Concept - Office/R&D . . . . .	96
24. Pedestrian Access in Large Parking Areas . . . . .	99
25. Pedestrian Access between R&D and Village Commercial . . . . .	100
26. Hardscape Elements . . . . .	102
27. Hardscape Elements . . . . .	103
28. Pedestrian Linkage . . . . .	104
29. Lighting Guidelines . . . . .	107
30. Screening . . . . .	110
31. Signage . . . . .	111
32. Signage . . . . .	114
33. Conceptual Landscape Plan . . . . .	116
34. Ventura Boulevard - Streetscape #1 . . . . .	118
35. Camarillo Center Drive - Streetscape #2 . . . . .	119
36. Parking Area Landscape . . . . .	123
37. Edge Treatment #1 . . . . .	125
38. Edge Treatment #2 . . . . .	126
39. Service Corridor . . . . .	128
40. Edge Treatment #3 - Park/Outdoor Recreation/Retention . . . . .	129
41. Edge Treatment #4 . . . . .	130

<u>Figure</u>	<u>Page</u>
42. Edge Treatment #5 . . . . .	131
43. Project Entry . . . . .	133
44. Secondary Entry . . . . .	134

<u>Table</u>	<u>Page</u>
1. Planning Area Acreage . . . . .	34
2. Development Yield . . . . .	46
3. Zoning Classifications . . . . .	48
4. Maximum Building Heights . . . . .	72
5. Building Coverage . . . . .	73
6. Minimum Parcel Size . . . . .	74
7. Required Parking Ratios . . . . .	77
8. Materials Palette . . . . .	83



<b>Foreword</b>	<b>• 1.0</b>
<b>The Prado de Las Posas Plan</b>	<b>• 2.0</b>
<b>Intent and Structure</b>	<b>• 3.0</b>



## 1.0

### FORWARD

The Prado de Las Posas Specific Plan has been prepared to carry out objectives of the Camarillo General Plan, while at the same time responding to market demand and individual development proposals.

The land use regulations and guidelines contained herein have been developed to achieve a character and quality for future development through site planning, architecture, signage, landscape and hardscape design concepts.

Taken together, the regulations and guidelines are intended to provide a framework for project design. Innovation is encouraged. Common features of site development will be controlled throughout the project to provide unity and express a strong collective design statement. At a site-specific level, individual projects will be required to comply with planning and site design guidelines specifically applicable to each use. Unique identities for individual projects with respect to tenant preferences and market factors may be allowed, within the framework of the specific plan.

Summarized, the Specific Plan goals are as follows:

- Provide the City of Camarillo with a Specific Plan that will implement General Plan goals and objectives;

- Provide a project planning framework and clear design direction without limiting creativity;
- Establish specific criteria for use by planners, architects, landscape architects, engineers, builders and future property owners in pursuit of project approvals and entitlements;
- Provide guidance to the City Staff, Planning Commission and City Council for reviewing development projects within the project area; and,
- Provide guidelines to encourage quality development; and
- Provide guidance to avoid unnecessary delays in the development process.

The California Government Code authorizes cities to adopt Specific Plans either by resolution as policy or by ordinance as regulation. **This is a policy Specific Plan.** Development plans, zoning, site plans, and tentative parcel maps or tract maps shall be consistent with this Specific Plan.

The overall purpose of this Specific Plan is to provide a link or "bridge" between the General Plan, with its City-wide goals and policies, and the detailed procedures and development regulations that are part of the Zoning Ordinance. The Specific Plan focuses General Plan policy

on a precise area, with the intention of implementing that policy. Thus, this Specific Plan has been prepared to stimulate responsible design through customized regulations and guidelines.

## **2.0**

### **THE PRADO DE LAS POSAS PLAN**

The Prado de Las Posas Specific Plan includes 61.3 acres of property located east of the Camarillo Airport and adjacent to the Ventura Freeway. Specifically, the property is at the southeast corner of Ventura Boulevard and Las Posas Road. Proposed land uses will contribute to anticipated growth in the Camarillo Airport area.

In addition, development of the Prado de Las Posas Specific Plan should blend with the adjacent Leonard Specific Plan area to the east. Common landscape, architecture and circulation elements are to be uniformly applied throughout both developments in order to eliminate the perception of property boundaries.

The Specific Plan proposes to develop the property into a high quality commercial development with a mixture of uses that combine office, service commercial, research and development (R&D), retail, restaurant and hotel uses, plus the possibility of outdoor recreation uses (please refer to the Land Use Plan).



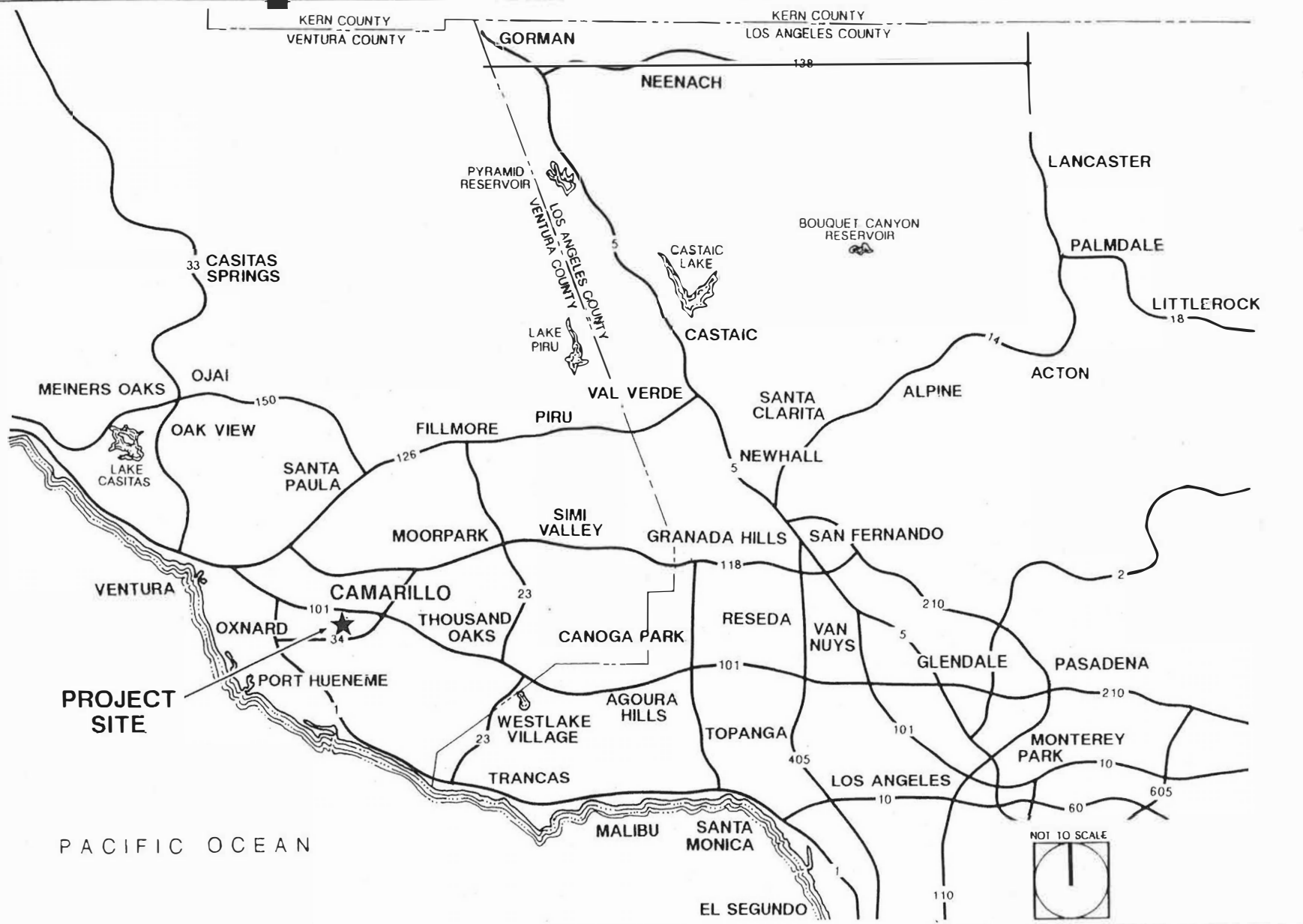


FIGURE 1 Regional Location Map







### 3.0

#### INTENT AND STRUCTURE

The Specific Plan has a dual purpose. First, the document establishes development entitlements for the subject properties. Second, it provides building standards, design guidelines and infrastructure plans necessary for the orderly development of a retail/commercial/office mixed-use area. The EIR is a companion document to the Specific Plan. This document contains detailed studies and findings about specific issues including traffic impacts and mitigation measures.

The Specific Plan is divided into six sections and will be adopted by resolution as a policy document. The *Summary* and *Introduction* sections present an overview of the planning area and describes its location, existing conditions, background and objectives of the plan and existing municipal land use designations. They also include statements of CEQA compliance and authority.

The *Development Plan* identifies opportunities and constraints and sets forth the concepts for land use, circulation, drainage, sewer, water and utilities. Consistency between each concept and parallel elements in the General Plan are discussed.

The *Development Regulations* section establishes criteria that must be observed by all Specific Plan projects. It covers site development standards including building

coverage, parcel size, setbacks, parking requirements and provisions for individual planning areas.

The *Design Guidelines* establish policy on the appropriate "look" for development plans. The guidelines cover landscaping, site planning, architecture, signage, parking, street furniture, trash enclosures and lighting. They are not intended to limit design creativity, but rather to encourage a coherent overall design that will help provide a sense of place and identity for developments within the Specific Plan area. This section uses extensive illustrations and narrative to depict aspects of development which must be addressed by each development to assure continuity.

The *Implementation* section covers four critical aspects of project implementation: phasing, comprehensive maintenance, processing and procedures, and General Plan consistency. Within the Processing and Procedures chapter, the rules for processing development plans and, if necessary, amending the Specific Plan are specified.

The overall purpose of this Specific Plan is to provide a link or "bridge" between the General Plan, with its City-wide goals and policies, and the detailed procedures and development restrictions inherent in implementation tools such as zoning. The Specific Plan attempts to focus broad policies of the general plan on a precise area with the intention of implementing that policy. Thus, this Specific Plan has been prepared to stimulate responsible design through customized policies and guidelines.

**Location and Description • 1.0**

**Existing Conditions • 2.0**

**Background and Purpose • 3.0**

**CEQA Compliance and Authority • 4.0**



1.0

**PROJECT LOCATION AND DESCRIPTION**

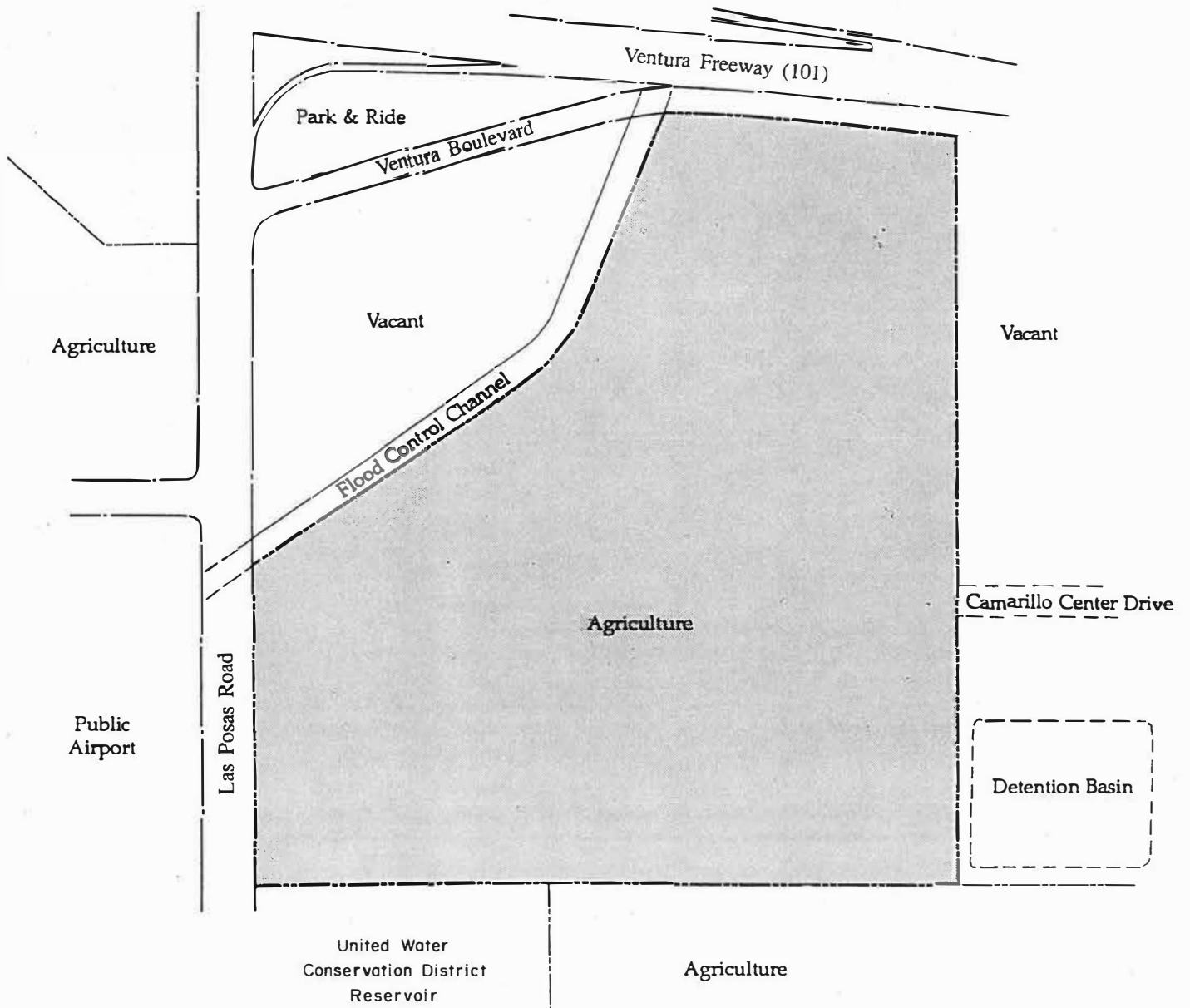
The Prado de Las Posas Specific Plan has been prepared for 61 acres of property located east of the Camarillo Airport and adjacent to the Ventura Freeway. Specifically, the property is at the southeast corner of Ventura Boulevard and Las Posas Road. Proposed land uses will contribute to growth in the Camarillo Airport vicinity. The Camarillo Airport and Airport North Specific Plan area are just west of the project site. The Ventura Freeway and Ventura Boulevard are adjacent to the northern property boundary. Future plans for Ventura Boulevard include its realignment which will bisect the project site. The Koll Leonard Specific Plan Area is adjacent to the eastern site boundary. Property south of the site has agricultural zoning and is currently under cultivation. In addition, there is a groundwater recharge basin and a pump house adjacent to the southwest corner of the site. The pump house is a Pleasant Valley County Water District service facility and the reservoir is the United Water Conservation District facility.

Development within the Prado de Las Posas Specific Plan area will be compatible with development in the adjacent Koll Leonard Specific Plan area. Common landscape, architecture and circulation elements are to be uniformly applied throughout both projects in order to eliminate the perception of a property boundary. The Koll Leonard Specific Plan was approved in 1985. Since that time, a backbone infrastructure has been installed, roads have been

constructed and landscape has been planted. Off-site storm drainage improvements and widening of Ventura Boulevard adjacent to the site have also been completed. In March of 1991 a Specific Plan amendment for the portion of the Koll Leonard site between Camarillo Center Drive and Ventura Boulevard was approved by the City resulting in an expansion of uses which included a potential for greater retail usage. More recently a 12-screen theater complex (CPD-176) was approved south of Ventura Boulevard and west of Plaza de Flores and a 250,269 square foot factory outlet mall (CPD-171) was approved south of Ventura Boulevard and east of Plaza de Flores in the Koll Leonard Specific Plan area. To maintain design continuity between the adjacent developments, the Koll Leonard Specific Plan and the Prado de Las Posas Specific Plan, the Koll Leonard *design guidelines* shall be incorporated.

The Prado de Las Posas property is presently utilized for agricultural purposes and the current General Plan designation is for agriculture. Due to the size of the site and its location, agriculture is considered an interim use and the designation will be changed through the General Plan Amendment and Specific Plan process.

The Prado de Las Posas Specific Plan proposes to develop the property into a high-quality retail business park, with unique development patterns capable of accommodating large and small-scale retail, office, service commercial and R&D uses and the potential for outdoor recreational uses.

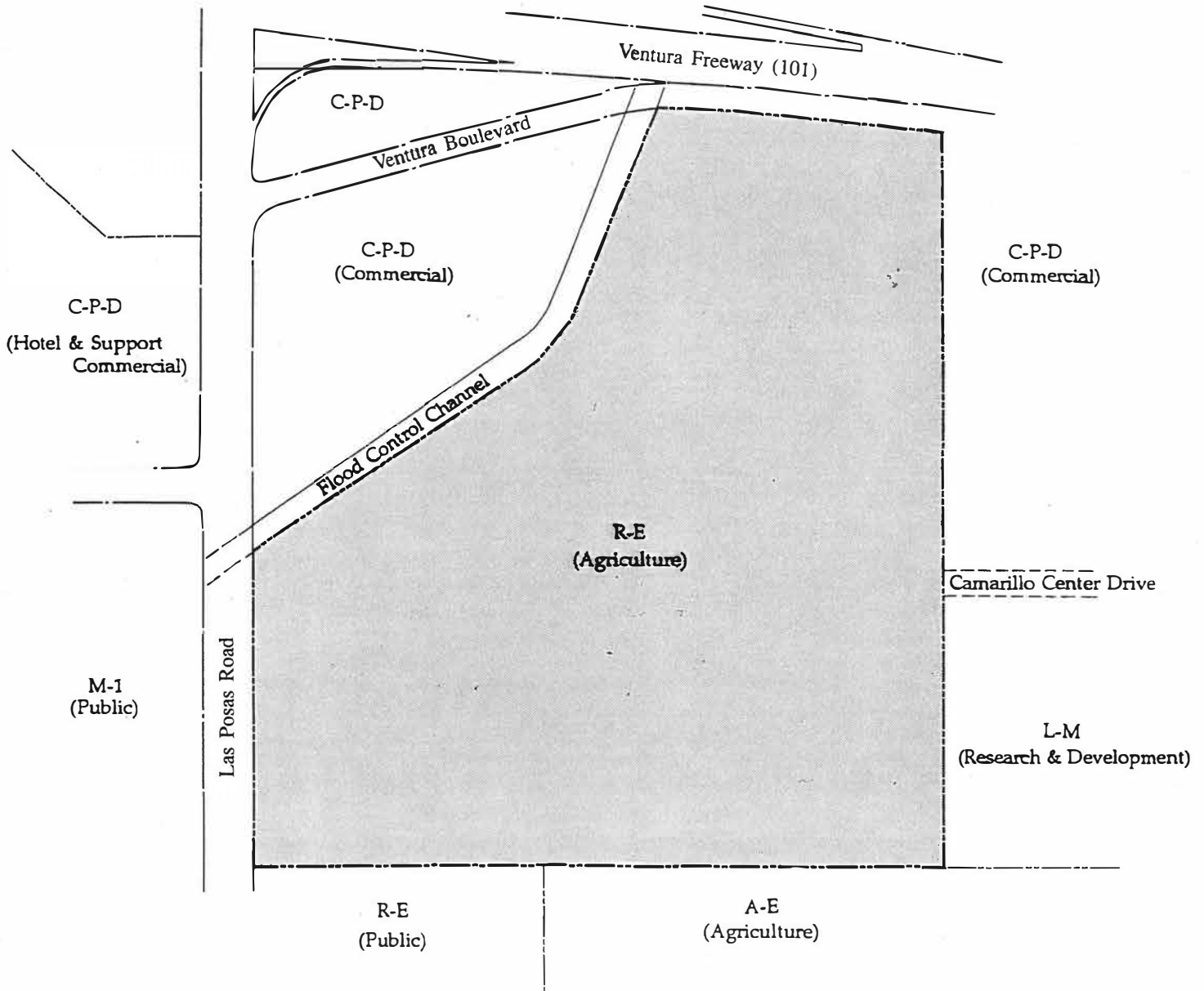


SCALE: 1"=400'



FIGURE 3 Existing Land Uses





**Zoning Designations**

- A-E AGRICULTURAL EXCLUSIVE ZONE
- R-E RURAL EXCLUSIVE ZONE
- P-O PROFESSIONAL OFFICE ZONE
- C-P-D COMMERCIAL PLANNED DEVELOPMENT ZONE
- L-M LIMITED MANUFACTURING ZONE
- M-1 LIGHT MANUFACTURING ZONE

General Plan Designations are in parenthesis

SCALE: 1"=400'

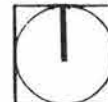


FIGURE 4 General Plan and Zoning Designations



The primary circulation system is a "bent grid" forming a parallel east-west alignment of Ventura Boulevard and Camarillo Center Drive. Private internal roads will connect Ventura Boulevard and Camarillo Center Drive.

This circulation system has a dual function. First, it divides the traffic flow between three specific plan areas (Airport North, Koll Leonard and Prado de Las Posas). The intent is to meter traffic flow through two major intersections to Las Posas Road and the Ventura Freeway (US 101). Second, it provides access to and internal circulation for the Prado de Las Posas Specific Plan area.

The Camarillo General Plan identifies Ventura Boulevard as a scenic drive. Therefore, this Specific Plan requires special treatment of the landscaped parkway, sidewalks and planted median. These special parkway treatments will be consistent with the Airport North and Koll Leonard Specific Plan areas to the west and east.

Four planning areas surround the grid with a mixture of proposed land uses that emphasize retail, office and service commercial activities. A high-profile land use such as a hotel, fine restaurant, corporate office or other retail development will be located near the northerly site access. Developments in this northernmost planning area will be carefully scrutinized and subject to specific architectural requirements. A landmark feature in this area is intended to establish a strong visual identity for the project. The landmark feature may take the form of a clock tower,

sculpture, special landscaping or a dramatic architectural treatment. The Specific Plan also proposes retail, office and restaurant uses in planning areas adjacent to the Ventura Boulevard re-alignment. Corporate offices and research and development uses are proposed for the southeast portion of the site. The western planning area is an outdoor recreation area for low intensity uses such as an executive golf course, open space uses or retail/wholesale nursery. This area will act as a buffer between the airport and project area and a portion may serve as a stormwater retention basin. The southern edge of the Camarillo Hills Drain forms a project boundary which will require a special edge treatment to soften the channel's visual appearance. Adjacent to this property boundary is a possible bus turn-out.

Pedestrian circulation is a focal element of the project that will help unify the scale, landscape and architectural themes. Standards and design guidelines support the pedestrian element, encouraging interactions between various uses on and off-site. Pedestrian circulation is a crucial design element that integrates the commercial village, outdoor recreation and office activities which builds a lively sense of place for the project and will help extend the hours of activity. In addition, emphasis on pedestrian circulation will facilitate a more self-contained mixture of land uses that provide jobs and services more efficiently, utilize resources more efficiently, generate less waste, create less air pollution and reduce transportation demands.

The City of Camarillo General Plan designates the land adjacent to the Ventura Freeway as being in the Heritage Zone area of the City. Architectural styles appropriate for this area include: Mission style, Monterey style, Early California style, Mediterranean style or modern interpretations of these styles. The most important aspect of the Heritage Zone is type of materials, color, texture and scale of architectural elements. The design guidelines require that all projects conform to Heritage Zone standards.

## **2.0**

### **EXISTING CONDITIONS**

#### **2.1**

##### **APPLICABLE PLANS AND POLICIES**

###### **Existing Land Use**

The subject area has been and is now in agricultural production. Current zoning is RE (Rural Exclusive) and RE-1 Acre (Rural Exclusive, 1 acre minimum), with the majority of the site RE-1. The RE designation is intended for preservation of large lot subdivisions capable of producing and supporting agricultural uses including the keeping of certain farm animals. The minimum parcel size in the RE Zone is 10,000 square feet, while the RE-1 Acre Zone requires a minimum one acre. Under the General Plan designation of agriculture, a minimum parcel size of 10 acres is required.

**Local and Regional Plans**

The project area is within the City of Camarillo with land uses governed by the City of Camarillo General Plan and the Airport Comprehensive Land Use Plan (CLUP). The SCAG Growth Management Plan and the Section 208 Water Quality Management Plan both provide a framework for regional goals and policies as well as regional growth forecasts. In addition, the Clean Air Act requires an Air Quality Management Plan for non-attainment areas, such as Ventura County.

**Airport Comprehensive Land Use Plan**

The CLUP addresses three main areas of public concern relevant to Camarillo Airport: aircraft noise, public safety, height and use restrictions. The CLUP incorporates noise contours developed as part of the Airport Noise Control and Land Use Compatibility Study and Master Plan Environmental Impact Report.

The Specific Plan site is outside of the 60-65 CNEL area, however, areas of Camarillo within the Airport Zone are designated for non noise-sensitive uses in the City of Camarillo General Plan. Uses considered compatible in the 60 to 65 CNEL zone include small retail and service establishments, office uses and R&D.

The CLUP public safety component addresses land uses in the Airport Zone. No structures are permitted within the primary surface or the runway plus 200 feet at each end. Clear zones are located immediately off each end of the

runway on airport property. The CLUP calls for the City of Camarillo to discourage activities that involve a large concentration of people such as theaters or schools in the clear zone. The project area is outside the clear zones.

The CLUP standards for height restrictions come from Federal Aviation Regulation Part 77 concerning obstructions in the navigable airspace around airports. Proposed development must comply with height restrictions specified in the airport easements acquired by the U.S. Air Force prior to conversion from military to civilian use. These height restrictions are incorporated in the Specific Plan guidelines.

#### **City of Camarillo General Plan**

The City's General Plan has numerous goals, principles and policies stating and implementing the City's support for the continuance of agricultural activities on high quality agricultural land. Camarillo wishes to separate residential urban land uses from agricultural land uses in order to avoid interference with efficient agricultural practices. For example, crop dusting, fertilization, and farm machinery operations when they occur near residences can lead to justifiable public safety and nuisance complaints from residents. The General Plan also envisions orderly infill development and the avoidance of leapfrog development in order to achieve efficient growth patterns and avoid urban sprawl.

**Section 208 Water Quality Management Plan**

The Section 208 Plan was issued in 1980 under the 1972 Clean Water Act and provides for regional water quality planning. The plan identifies water quality problems in the county and the measures taken to address those problems. The County of Ventura is the local lead agency for formulation and implementation of the plan which is now being updated. The plan is consistent with other county plans and uses the Board of Supervisors adopted growth forecasts to judge consistency.

Project impacts on water relevant to the Section 208 Plan relate to water use, waste water generation and storm water runoff. No significant impacts to water are expected from storm drainage and sanitary sewage.

**Air Quality Management Plan (AQMP)**

The AQMP specifies that projects must consider the Ventura County Air Pollution Control Board (VCAPCB) Guidelines for the Preparation of Air Quality Impact Analysis, adopted October 24, 1989. The air quality was reviewed as part of the environmental review and will be reviewed with each phase of development with appropriate mitigation, if necessary.

**2.2****GEOLOGY**

The site is located in Pleasant Valley, south of Camarillo Hills within the Transverse Range Province of Southern California. The surrounding hills and mapped geologic

features are typically oriented northeast/southwest, consistent with structural and topographic trends of the larger Transverse Ranges province. This east-west alignment of features is attributed to recent tectonic activity (within the last 10,000 years).

Geologic features on the property include a mapped anticline<sup>1</sup>, buried under alluvium along the eastern property line along with two faults mapped crossing the southern half of the parcel from east to west.

The 61-acre parcel is largely covered by poorly consolidated alluvium to an undetermined depth. Alluvial sediments are apparently underlain by Plio-Pleistocene sediments (a type of marine bedrock) of the Saugus Formation.

These alluvial materials typically consist of unconsolidated, unstratified to roughly stratified deposits of sand, silt, clay, gravel and boulders. These sediments, which were deposited by sheet flow and channel flow, are generally poorly consolidated and soft to moderately firm. Due to the porous loose nature of these sediments, some removal and compaction will be required to provide adequate foundation support. The marine bedrock crops out at the southeastern corner of the property. These sediments consist of fine to medium-grained sandstone.

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<sup>1</sup> A rock stratum or group of strata forming a bend with the convex side upward.

### **Surface Drainage**

At present, water falling as precipitation on the property flows as sheetwash, generally toward the southwest. Surface flow is experienced only during and shortly after periods of high intensity precipitation.

### **Groundwater**

It is anticipated that ground water levels may be within 20 to 50 feet of the present surface elevation. True groundwater depth should be confirmed during preliminary geotechnical investigations required for grading permit approval.

### **Seismic Activity**

The property, as with all of Southern California, is in an area subject to periodic earthquake-induced ground shaking. Two faults, identified in a report titled "Reconnaissance Geohazards Assessment, City of Camarillo," by Staal, Gardner & Dunne, Inc., (1987), have been mapped crossing the southern half of the parcel from east to west. At the present time, Camarillo considers these faults to be potentially active. Site specific geotechnical information including trenching and soil borings are needed to fully mitigate potential impacts from the two on-site faults. Further site review would be required before any entitlements for development are granted.

### 2.3

#### AGRICULTURE

The project site is located within the Oxnard plain, one of the most productive agricultural areas of the United States. Most of the site is prime farmland and the remainder is farmland of statewide importance, as mapped by the Farmland Mapping and Monitoring Program. Prime farmland has the "best combination of physical and chemical characteristics for crops"<sup>2</sup>. Most of the County crop production is from prime farmland or from farmland of statewide importance, which are the categories for irrigated high quality farmland. Rainfall has been too sparse in recent years for non-irrigated land to be highly productive.

### 2.4

#### CULTURAL RESOURCES

##### Archeology and History

Cultural and scientific resources include paleontological, archaeological and historical materials that provide information about the past of a particular geographic region.

##### Paleontological Resources

Paleontological resources refer to the plant or animal fossils from earlier geological periods (3.5 billion and 7,000 years ago). Typical paleontological resources

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<sup>2</sup> Advisory Guidelines for the Farmland Mapping and Monitoring Program, April 1984, California Department of Conservation, Division of Land Resource Protection.

include hardened remains or imprints from plants, invertebrates, such as insects or shellfish, or vertebrates including fish, amphibians, birds, reptiles or mammals. The existence of this type of resource provides materials that may increase scientific knowledge about the region in prehistoric times. Previous studies have not identified any paleontological sites within the specific plan area.<sup>3</sup>

#### **Archaeological Resources**

Archaeological resources include cultural artifacts, human burials, or any plant or animal remains found within a context that suggests human occupancy of the site in prehistoric times. An archaeological resource noted within the cultural context may provide scientific information about the indigenous inhabitants of a region.

A literature search was conducted on archaeological surveys in the project vicinity. There have been no archaeological surveys or excavations conducted on the Prado de Las Posas Specific Plan site. Four surveys and/or excavations have been conducted within a one mile radius of the site.<sup>4</sup> One survey (1978) adjacent to the southwest corner of the property yielded registered archaeological sites.

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<sup>3</sup> Letter from Brian Glenn, California Archaeological Inventory, Regional Information Center, 08/20/90.

<sup>4</sup> Ibid.

## 2.5

### CIRCULATION

This section summarizes information in the technical traffic study prepared by Linscott, Law & Greenspan, Engineers. The study is reproduced in its entirety in Appendix B of the Environmental Impact Report.

Regional access to the project site will be provided primarily by the Ventura Freeway (US 101) which generally runs east and west in the vicinity of the project. This 6-lane facility is located adjacent to the north property boundary. Access from the Ventura Freeway is provided to the project at freeway interchanges with Las Posas Road and Carmen Drive. Las Posas Road and Carmen Drive are two key north-south arterials, west and east of the proposed project, respectively.

Daily Drive and Ponderosa Drive are classified as secondary arterial streets which parallel the Ventura Freeway to the north. Their east-west alignments serve to connect Las Posas Road with Carmen Drive. Other principal local streets that serve the Prado de Las Posas Specific Plan project site are Ventura Boulevard and Pleasant Valley Road.

## 2.7

### AESTHETICS/VIEW

The proposed specific plan would change 61 acres of agricultural land to a mix of urban uses including village commercial, commercial, office, research and development

and outdoor recreation uses. This would alter the existing visual character of the site itself as well as views from the Ventura Freeway, Ventura Boulevard and Las Posas Road.

The specific plan site is located in an area designated as the Heritage Zone under the Community Design Element of the General Plan. The Heritage Zone applies to all developments within the freeway corridor through the City of Camarillo and requires all projects to have a particular design theme. Architectural styles appropriate for this area include Mission style, Monterey style, Early California style, Spanish or modern interpretations of these styles. Buildings should provide for a variety of features paying careful attention to materials, colors, textures and the scale of architectural elements. A balance between building mass and scale should be achieved. Implementation of these concepts into the specific plan is intended to encourage visual interest allowing views to penetrate into the site. Incorporation of the Heritage Zone standards will provide for design continuity along the freeway and sets forth the character of the community.

### **3.0**

#### **BACKGROUND AND PURPOSE**

The Prado de Las Posas Specific Plan, establishes the general type, location, and character of development while allowing for creative design and flexibility within each development area, consistent with the Plan's overall framework. The Plan focuses on providing a sensitive

integration of urban and natural amenities within the plan area and establishes development controls to provide the City and individual land developers with the assurance that complete development of the project area will be as envisioned at the time of Plan adoption.

The specific plan process provides the City and landowners a role in the planning process. This process should foster coordination of public and private planning efforts and establish a cooperative partnership between developers, property owners and governmental officials to define and implement planning programs that address local needs, while at the same time supporting goals and policies embodied in the General Plan.

A specific plan differs from a zone change or a General Plan amendment in several ways. A specific plan indicates what zoning classifications will be used and where. It can also include development regulations, street alignments, design guidelines, land uses and other criteria for the orderly development of that area. A zone change will be required prior to the approval of any planned development permit (refer to Section VI, Implementation). This is the means by which the specific plan is implemented, by assigning a legal classification restricting the use of the property based on particular zones.

**4.0**

**CEQA COMPLIANCE AND AUTHORITY**

The Prado de Las Posas Specific Plan has been prepared pursuant to provisions of Section 65450 through 65453 of the California Government Code. This Specific Plan, if adopted by the City of Camarillo will serve as a guide for future growth and development of the project area. It is intended that this Specific Plan, through its maps, illustrations and text, will incorporate all the regulations and development standards affecting use of land within the Specific Plan area, and reflect the interest and concerns of City officials and property owners through these standards and regulations. The Prado de Las Posas Specific Plan will assist in implementing the City's General Plan.

**Opportunities and Constraints ● 1.0**

**Land Use Plan ● 2.0**

**Circulation ● 3.0**

**Outdoor Recreation ● 4.0**

**Drainage ● 5.0**

**Water ● 6.0**

**Sewer ● 7.0**

**Utilities ● 8.0**



## 1.0

### OPPORTUNITIES AND CONSTRAINTS

This section summarizes the development constraints and opportunities that were identified during the early stages of plan formulation. The identification of these concerns has been a factor in developing the plan and the development standards and guidelines.

## 1.1

### OPPORTUNITIES

#### On Site

Topography is flat due to the existing row-crop. Cut and fill will be balanced on-site, thereby eliminating transport of soils off-site.

An 8-inch water main serving the abandoned liquor store and restaurant may be used to service the northern planning area.

A 24-inch sewer line runs along the channel, off-site toward Las Posas Road, where it goes south to the Camarillo treatment plan. Sewage main improvements required to serve the project can link into this trunk line.

Close proximity of the Ventura Freeway gives the site excellent visibility. In addition, direct access to the freeway exposes the site to a large volume of pass-by traffic and gives a short travel distance for vehicles exiting for the freeway.

Ventura Boulevard will be realigned to bisect the site and is anticipated to be a heavily traveled arterial; thereby, bringing more through traffic.

Similarly, the site will also be visible to high volumes of traffic on Las Posas Road (a primary arterial) and Camarillo Center Drive (a secondary arterial).

#### **Off Site**

The Ventura Freeway (US 101) is a main regional transportation corridor. Freeway visibility and a short travel distance to the site are strong assets.

Construction of approved specific plans east and west of the site (Koll Leonard and Airport North, respectively) will generate business opportunities for Prado de Las Posas. The region in general is also experiencing economic growth.

As proposed, the project has direct access from the Koll Leonard Specific Plan area to the east along Camarillo Center Drive and Ventura Boulevard.

The site has secondary access to Las Posas Road.

#### **1.2**

#### **CONSTRAINTS**

##### **On Site**

A change in land use and subsequent development of the site will cause a permanent loss of agricultural land.

Virtually all major infrastructure (roads, gas, water, sewer, electric etc.) on site may be required to be either relocated and/or linked with surrounding systems.

Intensity of development is based primarily on the capacity of the adjacent roadway network.

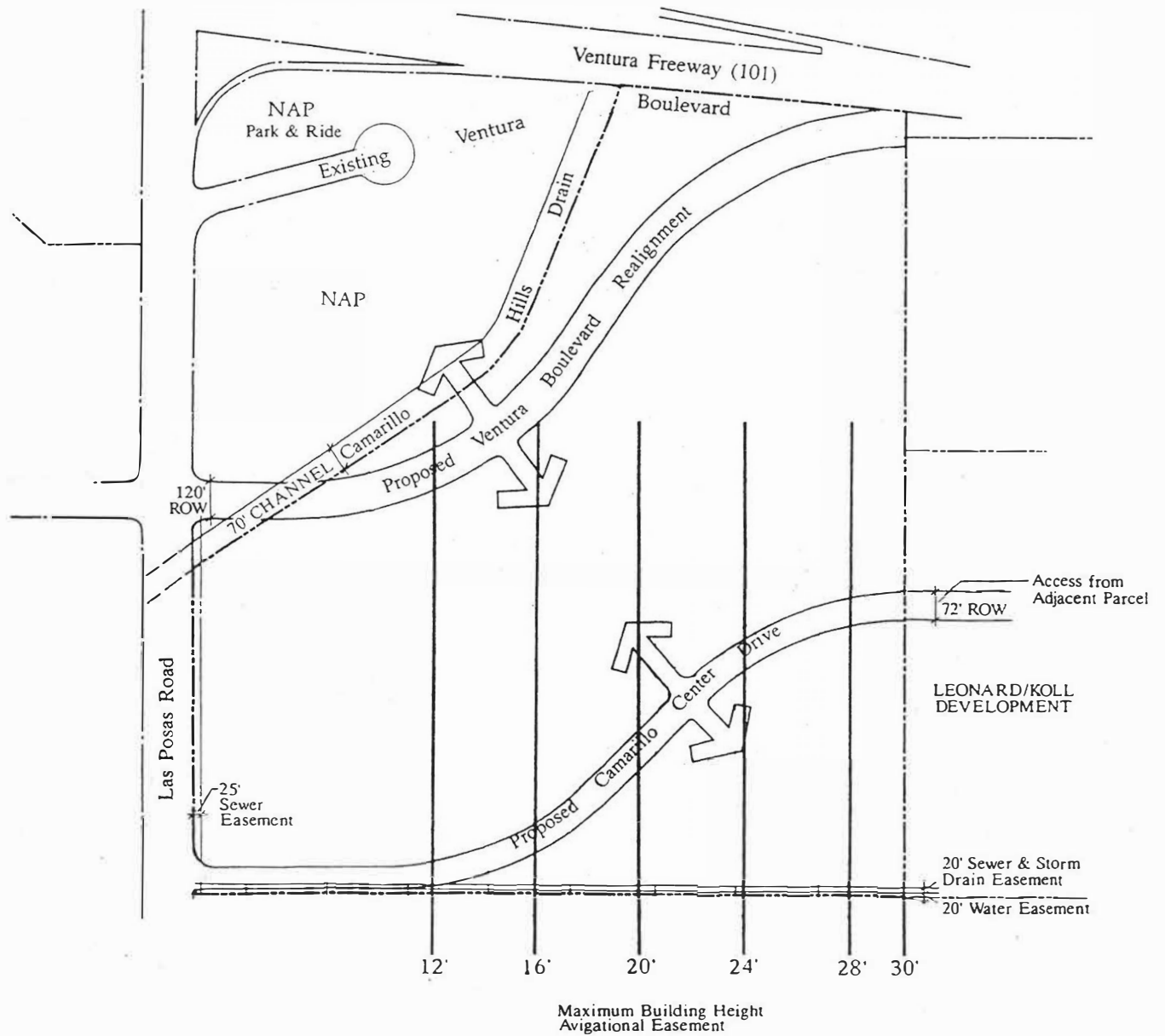
Avigational easements from the nearby Camarillo Airport create an envelope blanketing almost the entire site. This envelope limits the height of any structure or light standard from west to east across the site. See Figure 5.

#### **Off Site**

Presently, Ventura Boulevard adjacent to the site does not meet the City standards. Las Posas Road, Ventura Boulevard, Camarillo Center Drive and their respective intersections must be improved to meet City standards in order to implement the Prado de Las Posas Specific Plan. These circulation and infrastructure improvements are part of the City's larger improvement program for the areas south of the Ventura Freeway.

Initially, the developer will be required to construct or pay for the cost of all roadway improvements within and adjacent to the site. Specifically, this includes Las Posas Road adjacent to the site, Ventura Boulevard and Camarillo Center Drive within the site and signalization on Las Posas Road at Ventura Boulevard and Camarillo Center Drive. Additional costs for Las Posas Road improvements not adjacent to the site, may be reimbursed to the Developer(s) on a pro rata basis.





Maximum Building Height  
Avigational Easement

FIGURE 5 Opportunities and Constraints



Noise generated around the site will be from the adjacent roads and Ventura Freeway. Intermittent aircraft noise will have some impact on the site.

## **2.0**

### **LAND USE PLAN**

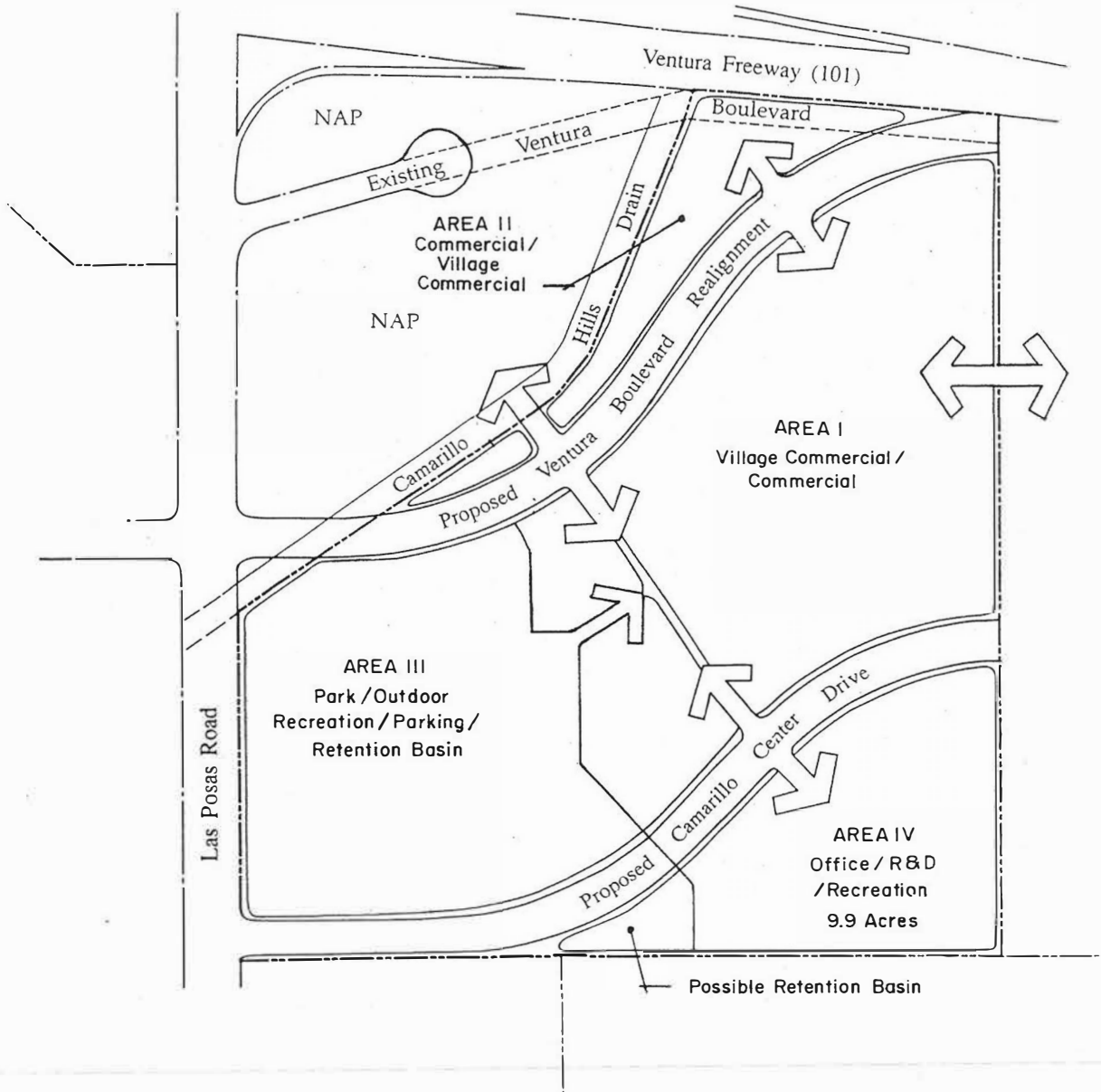
#### **Plan Structure and Concept**

The land use plan proposes an arrangement of uses within defined "Planning Districts." The boundaries of the individual districts and their associated land use and design concepts are derived from physical constraints such as the height restrictions imposed by the glidepath for the adjacent Camarillo Airport and existing land use and infrastructure conditions.

The proposed concept envisions a flexible plan; one capable of several development scenarios. One potential concept would have a commercial center with large scale anchor tenants. Smaller retail uses and restaurants with commercial and/or village commercial uses north of Ventura Boulevard and village commercial uses south of Ventura Boulevard. Office and R&D uses would be located south of Camarillo Center Drive.

The Plan proposes commercial and village commercial uses along Ventura Boulevard frontage where visibility from Ventura Freeway (US 101) is assured. The strongest possible project image will be presented through visibility of the "higher-end" commercial uses including restaurants and lodging.





SITE STATISTICS

LAND USE	ACRES
Village Commercial/Commercial Areas I & II • Services • Retail • Restaurant • Office	27.6
Park / Outdoor Recreation Area III	18.8
Office / R&D Area IV	9.9
Roads	5.0

TOTAL 61.3

SCALE: 1"=400'

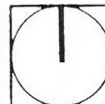


FIGURE 6 Land Use Plan



Private walkways through the site shall be utilized to create a pedestrian core, linking on-site focal points with features such as special paving and lighting, pergolas (trellis covered walkways), sculpture, fountains, and an auto core. Buildings fronting the pedestrian spine will be oriented in its direction. Limited parking may be permitted directly adjacent to businesses fronting the spine with breaks between buildings to allow traffic circulation to larger ground-level parking plazas. Parking plazas will be designed so that they can readily serve as open-air gathering sites for special promotions or seasonal events.

The Design Guidelines portion of this Specific Plan shall provide continuity between this project and the Leonard/Koll Specific Plan to the east. The Prado de Las Posas Design Guidelines shall follow the guidelines implemented in the Koll Leonard Specific Plan amendment area. The pedestrian spine will extend across both projects; and, thereby provide a linkage between both sites. The architectural theme shall be consistent between both sites and service vehicles may utilize a shared north-south service drive between the two developments. Customer and service vehicles may travel between both sites without utilizing Ventura Boulevard or Camarillo Center Drive.

The village commercial designation allows the developer a variety of options in responding to changing market demands through time. Activities permitted within this category include small offices, restaurants, retail and other support commercial uses;

The commercial designation allows uses identical to the village commercial designation with the addition of large-scale anchor retail users, restaurants, hotels/motels, offices and a range of other uses under the CPD Zone.

A combination of building placement, Heritage Zone architecture and an extensive landscaping program will present a dynamic visual image from Ventura Boulevard and the freeway. Massing of buildings will be controlled in such a manner that preserves views into the site from the Ventura Freeway and Ventura Boulevard. Larger structures will have their mass oriented away from the freeway and Ventura Boulevard.

The Office/R&D uses proposed south of Camarillo Center Drive will include a variety of "high tech", corporate offices, light manufacturing and assembly, distribution and research businesses.

#### **PLANNING AREAS**

The Prado de Las Posas Specific Plan area has been divided into four planning areas based on proposed land uses. Development standards for each planning area will be defined.

A statistical summary of net acreage by Planning Area is shown in Table 1.

Planning Area	Acreage	% of Site
I & II Village Commercial/ Commercial	27.6	45.0
III Outdoor Recreation	18.8	30.6
IV Office/R&D	9.9	16.2
V Roads	5.0	8.2
<b>Total</b>	<b>61.3</b>	<b>100.0</b>

## 2.1

### GOALS

#### Specific Plan Land Use Goals

The Prado de Las Posas Specific Plan ensures that the proposed land uses form a balanced and cohesive project that: (1) provides service opportunities, (2) reduces travel for goods and services for City residents, (3) is economically viable, (4) sensitive to environmental concerns and community aesthetics, (5) reflective of the City's general plan policies and development standards, (6)

can be implemented in a timely and efficient manner, and (7) can be adequately served and maintained by responsible agencies.

Specifically, the plan is designed to achieve the following:

- Respond to the growing demand for commercial services and business park space in the Camarillo region.
- Establish design and development standards that will insure a high quality project that is visually and functionally cohesive.
- Allow enough flexibility for adjustment to future market trends.
- Phasing of land use and infrastructure to assure that development will occur in an orderly, integrated and cost effective manner.
- Encourage the location of retail uses which will retain tax dollars within the City of Camarillo.

## 2.2

### SPECIFIC PLAN

The land use designated on the southerly side of the Ventura Freeway, northerly of the Camarillo airport, westerly of Las Posas to Central Avenue has been approved for the Airport North Specific Plan. Other specific plans include the civic center,

Koll Leonard, Rancho Calleguas Specific Plan and the Pitts Ranch Specific Plan.

Properties designated as "urban reserve" allow for consideration of specific plans as do other areas which propose conversion from an agricultural use. Areas of consideration during review of the specific plan should include: maintenance of the open feeling which presently exists in Camarillo through the provision of adequate building separations, expansive setbacks, rolling landscapes and pedestrianways, all in keeping with the quality and standards established by the City. Specific plans should also address phasing, implementation and design criteria.

The Prado de Las Posas Specific Plan proposes conversion of an agriculturally designated property to urban uses. The proposed uses, concepts, design standards and design guidelines are consistent with the quality and standards established by the City.

### 2.3

#### **PERMITTED USES**

##### **1. Commercial Center**

Commercial center uses may include retail, commercial services, offices and restaurants. These uses are compatible with each other and will help support activities within the Specific Plan area. Proximity will provide opportunities for interaction and competition. Commercial center uses would be allowed in Areas I & II. For example, a duplication service (Xerox,

blueprinting) will provide a needed service for other commercial and retail businesses, offices, and R&D uses. A courier service may provide additional opportunities for interaction between the duplication service, offices and R&D uses within the specific plan area and within the region.

*Service Commercial* -- Examples of these uses include, but are not limited to, duplication services, dry cleaners, beauty salons, tailors and health clubs. Additional services allowed, but not directly dependent on other permitted uses, include automotive diagnostic, car washes, child care and veterinaries.

*Retail* -- The Prado de Las Posas Specific Plan is designed to accommodate two distinct categories of retail uses. Large shopping facilities with a regional orientation and smaller support retail businesses.

The large retail anchor stores (approximately 75,000 to 100,000 s.f.) are permitted in portions of Planning Area I with placement depending on height restrictions. Smaller retail establishments will augment the large anchor retail users and serve employees in and near the development. Examples of such "smaller" uses include computer stores, camera shops, florists, stationary supplies or pharmacies. These uses are compatible with both the Commercial and Village Commercial design concepts..

*Restaurant* -- Establishments ranging from "high-end"

restaurants to food parks, bakeries and sandwich shops are appropriate for the Village Commercial design theme. "High-end" restaurants are envisioned in or near Planning Areas I and II, while smaller, less formal establishments may locate in Planning Area III. Drive-through restaurants are not encouraged.

Commercial services, restaurants and retail required to support a major business center will be encouraged within the Village Commercial planning areas.

## 2. Hotel

Landscaping and architectural treatment will emphasize the project entrance. A hotel would be a complement to on- and off-site office uses, research and design activities, retail, restaurants and commercial services. In addition, proximity to the Camarillo Airport and the Ventura Freeway combined with continued growth in the region make Prado de Las Posas an appropriate site for a hotel. Planning Areas I or II will permit hotel development.

## 3. Office

Office uses ranging from corporate to single tenant offices would be permitted in the commercial and R&D areas. In particular, firms seeking a pleasant working environment in a location which has prestige will be attracted. Applicable general design and development standards have been established to promote a professional image.

Other uses in this category shall be service oriented and may typically include small and medium sized medical practices, single tenant and small offices such as legal, accountants, design, financial services and real estate firms.

#### **4. Research & Development (R&D)**

This category will provide for a variety of "high tech", light manufacturing and assembly, distribution and research functions consistent with the following criteria:

- Manufacture, processing, blending and packaging of food and kindred products.
- Manufacture, processing, assembling and packaging of products or merchandise from previously prepared materials.
- Production and testing of parts, accessories, assemblies, systems, engines, components, electronic devices, aircraft, vehicles, and similar products.
- Manufacture, processing, assembling and packaging of precision components and products.
- Warehousing and distribution.
- Scientific and industrial research and development laboratories.

- Regional administrative or executive operation of an industrial establishment.

The R&D buildings shall complement design themes established in the Village Commercial areas. Well-landscaped, contemporary, spacious and articulated structures (up to two stories) shall be encouraged. All uses shall be fully contained within a building. No exterior operation or storage will be allowed; except parking, loading and receiving, which shall be screened. R&D uses will be permitted in Planning Area IV.

#### **5. Outdoor Recreation**

This designation applies specifically to Planning Area III. This dual purpose Planning Area serves in part as a possible retention basin to meter drainage flow from the site and as a buffer zone offering a highly aesthetic transition to nearby airport uses. Outdoor recreation and associated uses such as a clubhouse or pro shop are permitted in Planning Area III subject to avigational height restrictions.

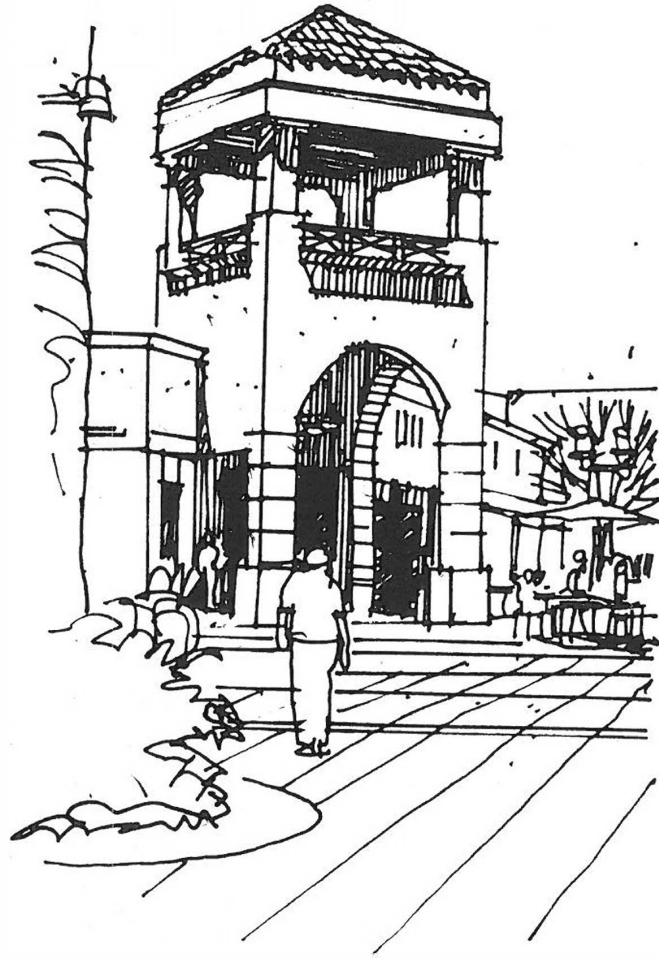
Commercial service, office and commercial retail uses are excellent examples of the interdependence and long-term flexibility promoted by mixed use planning policies. Commercial service uses have the potential to offer and draw support from office retail and R&D uses, both on site and in the surrounding area. A similar network of interactions will develop between hotel, retail, office and R&D uses.

In addition, the flexibility of a mixed-use policy allows for the transition of land uses in response to economics of development over time.

**Planning Area I (Village Commercial/Commercial)**

This planning area is designed to accommodate large shopping facilities with a regional orientation, support retail uses and related commercial businesses, all within a Village Commercial concept. Buildings are envisioned as low mass and may be clustered, having a sense of pedestrian orientation. Features should include pedestrian spaces and paths and generous amounts of lush plant materials.

The area has the potential for two large anchor stores. Smaller retail establishments will augment the large anchor retail users and serve shoppers and employees in and near the development. Restaurants, hotels, commercial services and office uses are also permitted. The array of uses will be compatible with the Village Commercial design concept and are intended to offer a complete spectrum of opportunities. Buildings up to two stories will be permitted subject to avigational easements.





**Planning Area II (Commercial/Village Commercial)**

This planning area is located at a main entrance to the project and in its design is recommended to help establish a theme for the overall development. Architecture and other site feature shall allow for views to penetrate into the main site. Building scale, architecture, walls, landscaping and entry features shall work in concert to create a focal point in the development. Buildings should be low mass and well- articulated and may use such elements as a tower, fountain, sculpture or focal landscape feature to enhance and complement the architecture.

Special parkway and median landscaping along Ventura Boulevard will reinforce the importance of Planning Area II.

Hotels, restaurant, retail, commercial services and office uses are permitted.

**Planning Area III (Outdoor Recreation)**

This planning area will allow for low intensity uses such as an executive golf course or retail/wholesale nursery. It will also serve as a transitional use and buffer between nearby airport uses. A portion may function as a possible drainage retention basin to meter drainage from the site. Given the avigational height restrictions for this area, buildings will be low mass and at a smaller scale. This area may contain a special feature and landscaping that will visually enhance the project site and provide opportunities for passive recreation, such as walking and picnics.

The special feature may include a fountain, sculpture or other architectural feature. If a fountain is utilized, water must be recirculated. Any water feature shall not contain flowing water.

#### **Planning Area IV (Office/R&D/Recreation)**

The industrial/office use designation will allow light industrial and office uses ranging from corporate to single tenant offices and firms seeking an attractive and pleasant working environment plus a location that has prestige. Applicable general design and development standards have been established to promote a professional image.

This planning area will also permit a variety of R&D "high tech", light manufacturing and assembly, distribution and research functions. Low-rise buildings (one and two stories) consistent with the airport easements in clustered landscaped settings are envisioned. Architectural themes with contemporary, spacious and articulated structures are encouraged.

Office and R&D uses are permitted. In addition, recreational uses may be permitted in this area subject to approval of a Zone Ordinance amendment which would allow for recreational uses in the R&D areas (LM Zone) subject to the approval of a conditional use permit.

#### **2.4**

#### **LAND USE SUMMARY**

The Land Use component of the Specific Plan has two parts: **The Land Use Plan** itself, which designates

conceptual locations of the project's proposed uses, and the **Development Yield**, which establishes the range of development intensity and shows alternate land use and densities in some locations.

#### **Development Yield**

The **Land Use Plan** specifies uses within each Planning Area. It is the graphic summary of the Specific Plan and provides a frame of reference for all of the Plan's regulations.

The **Development Yield** table specifies the maximum anticipated square footage of buildings permitted for each category of land use.

Individual project configurations and intensities may ultimately vary to respond to individual users, which could result in re-assigning intensities to other parcels within or between planning areas in order to maintain the total allowable square footage of development.

LAND USE	ACRES	% ACRES	BUILDING SQUARE FEET
AREAS I & II Village Commercial and Commercial	27.6	45.0	285,600
AREA IV Research & Development	9.9	16.2	151,316
AREA III- Outdoor Recreation	18.8	30.6	15,000
Roads	5.0	8.2	-----
<b>TOTAL</b>	<b>61.3</b>	<b>100.0</b>	<b>451,916</b>

### Special Conditions

Planning area boundaries may be adjusted with a resultant maximum acreage increase or decrease based on final project design. This may also result in square footage adjustments, subject to the following limitations:

1. The maximum of 451,916 square feet shall not be exceeded (61.3 gross acres);
2. Boundary adjustments may involve any adjacent planning area with the same zoning designation as the planning area in question, so long as all of the affected

land is owned and all parties can reach mutual agreement related to the final site plan.

3. Uses listed in the Development Yield table above and technical studies in the Environmental Impact Report represent the most intense development of the site. Less intense uses may be selected at the Planned Development Permit stage, providing their impacts are equal to or less than those analyzed in the Development Yield table.

For example, if Office/R&D entitlement is transferred into Retail use, there will be a corresponding decrease in square footage so that the net traffic impact will be equivalent.

### **Zoning**

The following zoning classifications will be used in conjunction with this Specific Plan:

TABLE 3

Zoning	Use
Commercial Planned Development (CPD)	Hotel Retail Restaurant Office Commercial-Services
Limited Manufacturing (L-M)	R & D Office

3.0

CIRCULATION

A traffic report was prepared for the site by Linscott, Law and Greenspan (LL&G) and is included in the Appendix. This report identifies existing traffic conditions, future traffic conditions and the distribution of project related traffic to roads inside and outside of the project. The study has identified road improvements that will be necessary to mitigate traffic impacts. Development plans in the Specific Plan area shall incorporate the intersection and roadway improvements to satisfy the Environmental Impact Report mitigation measures.

A main objective of the circulation plan is to provide an efficient and logical street system for Prado de Las Posas and surrounding vicinity. The resulting plan is a "bent grid" formed by parallel northeast-southwest alignments of

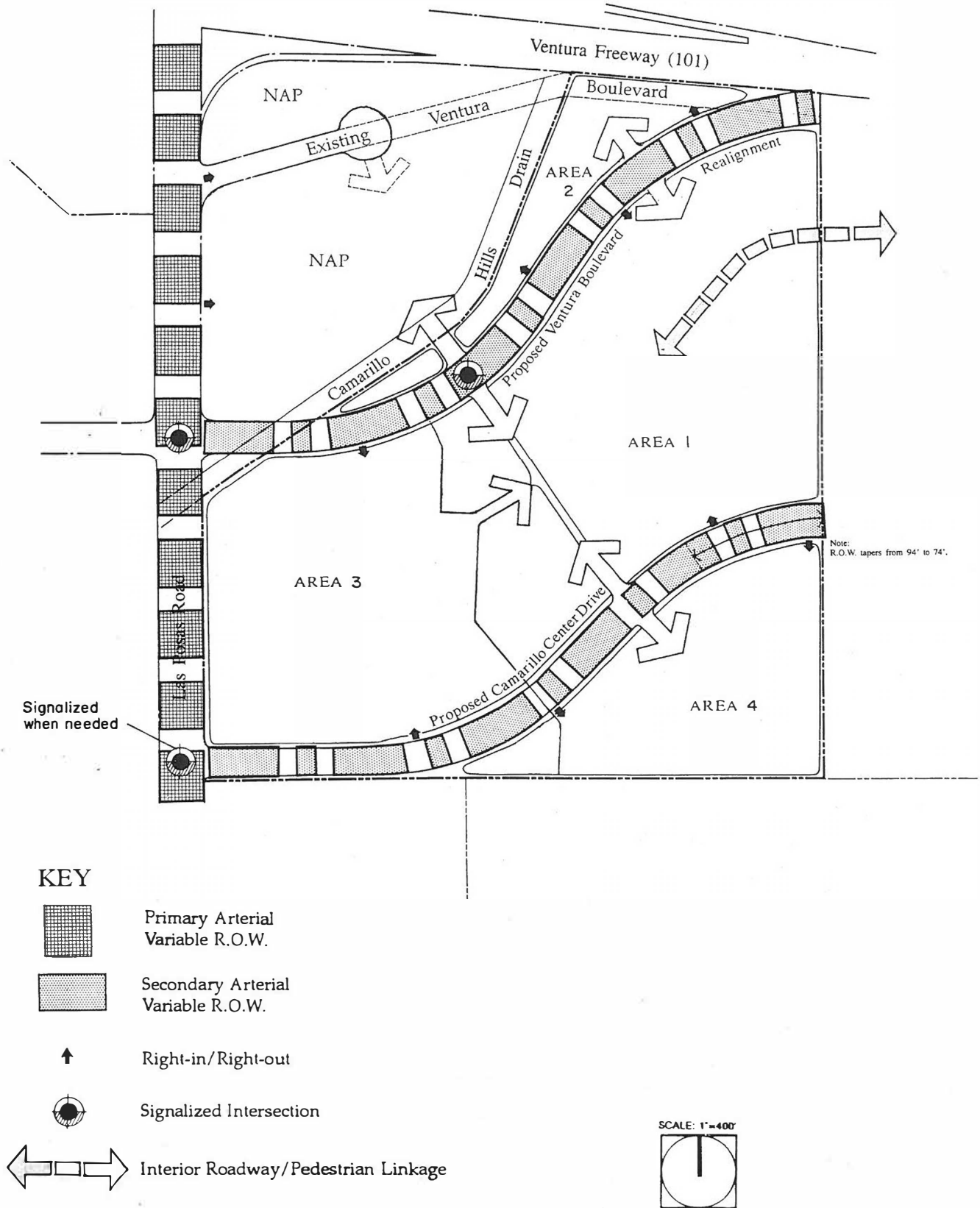


FIGURE 8 Circulation Plan



Ventura Boulevard and Camarillo Center Drive (see the following exhibit). Private internal roads will connect Ventura Boulevard and Camarillo Center Drive. This circulation system has a dual function. First, it divides the traffic flow between three specific plan areas (Airport North, Koll Leonard and Prado de Las Posas). The intent is to meter traffic flow through two major intersections to Las Posas Road and the Ventura Freeway (US 101). Second, it provides access to and internal circulation for the Prado de Las Posas Specific Plan area.

The developer will finance any required on-site roadway improvements that are identified, including but not limited to, Ventura Boulevard and Camarillo Center Drive to the intersections on Las Posas Road with Ventura Boulevard and Camarillo Center Drive. Further, the developer may be required to contribute a "fair share" of the cost of off-site roadway improvements as determined by the City.

### 3.1

#### GOALS

##### Specific Plan Goals

- Provide adequate capacity for the circulation system to accommodate traffic generated from the project, ambient growth and related projects.
- Develop bicycle and pedestrian networks that are integrated with project design, landscaping and vehicle circulation on and off-site.
- Incorporate the extended parkway landscaping required for Ventura Boulevard, Camarillo Center Drive and Las Posas Road.

- Utilize scenic highway standards for Ventura Boulevard.
- Provide a safe and aesthetically pleasing transition between Ventura Boulevard and the flood control channel.
- Provide safe and efficient vehicle movement entering, exiting and within the site by a series of private driveways.

#### **General Plan Circulation Principles**

The following text discusses principles of the General Plan Circulation Element that are consistent with the Prado de Las Posas Specific Plan.

*Protect street traffic capacities by controlling points of access from adjoining land and by restricting on-street parking when and where necessary.*

Initial planning for the project was contingent upon results of the technical traffic analysis. Intensity of development is limited by traffic generated from adjacent developments, ambient traffic growth and related project generated traffic. The use of medians, signals, and driveways with right-in-right-out access are utilized to control access. On street parking is not permitted on these segments of Ventura Boulevard or Camarillo Center Drive.

*Promote the beautification of streets and other corridors by developing and maintaining a tree planting and landscaping program that will best enhance the character of Camarillo and its environs.*

Landscaping in the pedestrian linkage, setbacks and parkways adjacent to the flood channel are used to beautify and reinforce the project character. The landscape plan, design guidelines and maintenance program that will preside over the entire specific plan area ensures a cohesive development and continual maintenance of individual projects as they are constructed through build out and beyond.

*Keep traffic on all streets in balance with the capacity of the circulation system by regulating the intensity of land use in conformance with the General Plan.*

Similar to the first General Plan circulation principle, intensity of development in the Prado de Las Posas Specific Plan is limited by remaining capacity in the circulation network after traffic generated by anticipated growth has been accounted for.

*Provide adequate, efficient, safe and attractive pedestrian walkways and bikeways between major generators, such as schools, parks, shopping areas and transit terminals.*

The circulation plan and design guidelines include components for movement of pedestrians and bicycles that is both safe and attractive. The design for Las Posas Road and Camarillo Center Drive includes bike lanes that will implement the City's Bicycle Plan. Integration of the pedestrian linkage within and between planning areas and

off-site will be required as part of the Planned Development permit and subsequent development plans.

*Investigate alternative modes of public transportation and develop a system of routes and terminals that will most efficiently and economically meet the needs of the City.*

The specific plan includes the possibility of a centrally located bus turn-out. Its location on a heavily traveled secondary arterial, in close proximity to the Park-and-Ride facility, could be used to promote use of carpools and buses. The Specific Plan also provides opportunities for additional bus stops on Ventura Boulevard and Camarillo Center Drive. Conditions of approval for Planned Development Permits and Development Plans may include implementation of Transportation Management Programs and trip/parking reduction requirements.

### 3.2

#### **STREET HIERARCHY**

Improvements shall be in conformance with technical cross sections shown on the Circulation Plan Exhibit and the Street Landscape Section Exhibits. A summary of recommended improvements follows.

##### **Secondary Arterial**

The Camarillo General Plan identifies Ventura Boulevard as a scenic drive; therefore, the Specific Plan requires special treatment including a landscaped parkway, meandering sidewalk and planted median. These special

parkway treatments will be consistent with the Airport North and Leonard Specific Plan areas to the west and east. An on-road bicycle lane located on the both sides of Las Posas Road shall be linked to the City-wide bicycle trail system. In addition, bicycle lanes shall be provided on both sides of Camarillo Center Drive with their linkage to the City-wide bicycle trail system to be considered during the next update of the bicycle master plan.

Secondary arterials have four lanes of traffic and shall include a landscaped median and parkways with sidewalks. Landscaping within the medians and parkways shall be designed so that trees, shrubs, mounding and other obstructions shall not restrict sight distance at driveways and intersections. The intersections at the ends of the main private driveway shall have median breaks with left-turn pockets into the project to allow full traffic movement. Median breaks may be signalized if warranted.

Deceleration lanes for right turn movements will be incorporated into secondary arterial street design. This allows traffic to slow down while turning without restricting traffic flow. Deceleration lanes may also be required in the private drive(s) where they intersect arterial streets. The need for deceleration lanes shall be determined by the City.

As Camarillo Center Drive enters Prado de Las Posas Specific Plan property from the east, it will be widened and improved from industrial collector standards to a secondary arterial. The transition will occur between the

east property boundary and the median break at the intersection of Camarillo Center Drive and the main private driveway.

#### **Bus Stops**

Bus stops shall be provided as needed on Ventura Boulevard and Camarillo Center Drive. The City must approve bus stop location and design prior to construction.

A bus turn-out could be created in the curve required for Ventura Boulevard. It would serve as a link for the Park-and-Ride and as a convenient arrival/departure point for commuters using and working in the project.

The size and location of the bus turn-out depicted in the Specific Plan is for illustrative purposes only. The presence of the bus turn-out in the Specific Plan indicates that one may be incorporated somewhere on site. It is possible the bus turn-out may be located on the north or south side of Ventura Boulevard, within the Planning Areas I, II or III or elsewhere in the project. The City shall approve the final location and design of any bus turn-out in the Specific Plan area.

The extent of commuter facilities and landscape provided will be a function of the final alignment for Ventura Boulevard. As depicted in the land use plan, the planning area may provide a bus turn-out, a bus shelter, benches, bicycle racks, pedestrian drop off and pick-up and dense landscape adjacent to the Camarillo Hills Drain.

**Private Drives**

Project design shall include indirect connection(s) between Ventura Boulevard and Camarillo Center Drive. The purpose of the private drives is to efficiently conduct traffic on and off and within the project site. However, their design shall include features such as: restricted street widths, speed bumps, curves, islands, over hanging landscape, and stop signs to discourage the use of private driveways for trips directly between Ventura Boulevard and Camarillo Center Drive.

Right-in, right-out access to secondary arterials between median breaks will be permitted, provided adequate separation between driveways is maintained as determined by the City.

**Traffic Signals**

Traffic signals will be installed on the secondary arterials at median breaks when warrants are met. Signals shall be synchronized with Las Posas Road and provide for left turn movements.

**Off-Site Improvements**

The EIR for this Specific Plan contains mitigation measures which are necessary to reduce traffic impacts on streets and intersections generated by this project and surrounding developments.

Improvements for Ventura Boulevard, Camarillo Center Drive, Las Posas Road and the freeway interchange at Las Posas Road shall be implemented as set forth in the Environmental Impact Report or as determined by the City.

#### 4.0

### OUTDOOR RECREATION

Planning Area III is proposed as an outdoor recreation area containing approximately 18.8 acres. This area occupies the western portion of the site south of Ventura Boulevard and is within the Outer Safety/Traffic Pattern Zone of the Camarillo Airport. This planning area acts as a buffer zone offering a highly aesthetic transition to nearby airport uses. Given the avigational building height restrictions, the plan envisions open space uses which could include a nine-hole executive golf course, driving range or putting greens. The golf course may include support buildings, such as a clubhouse or restaurant, located further to the east consistent with the height restrictions, lounge and banquet facilities.

Planning Area III may also contain a possible storm drainage retention basin located on the south side of Camarillo Center Drive. The retention basin is envisioned as a broad grassy swale holding water only during periods of peak storm flows. The ultimate size of the basin will be a function of its capacity and depth.

Landscape elements within adjacent Planning Areas I and IV, the flood control channel, pedestrian linkage and streetscapes shall be integrated with the outdoor recreation area.

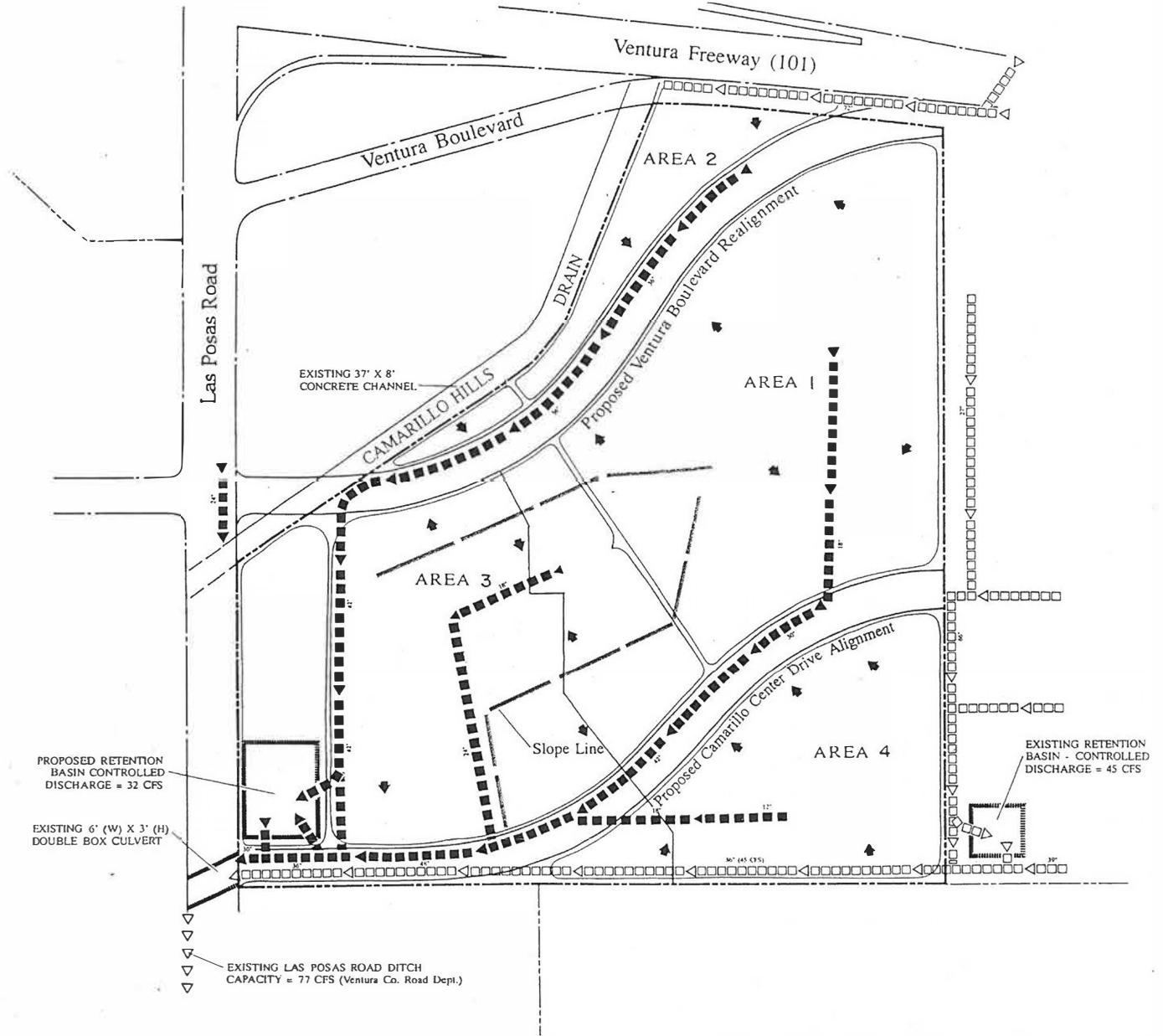
## 5.0

### DRAINAGE

The property slopes southwesterly at an approximate gradient of 0.7%. Drainage is by sheet flow to the southwest. Total grade differential across the site is 11 feet. A small portion of the site drains by gravity to the Camarillo Hills Drain, a major reinforced concrete channel existing along the northwesterly property line. This drain is under jurisdiction of the Ventura County Flood Control District (VCFCD). The predominate drainage pattern for the site is southwest, discharging to an existing double 6'(wide) by 3'(high) reinforced concrete box culvert (R.C.B.) crossing Las Posas Road. This culvert discharges to a roadside ditch running southerly in Las Posas Road (VCFCD) to the Pleasant Valley Road Drain (VCFCD), then west fronting the Camarillo Airport to the Revolon Channel which empties into Calleguas Creek and Mugu Lagoon.

Site drainage from the Koll Leonard Specific Plan subdivision is collected by on-site storm drains and discharged to a retention basin located just easterly of the southeast corner of this site. Discharge from this retention facility is controlled at 45 cubic feet per second (fps), which is transmitted via a 36" storm drain along the south boundary of this site, discharging to the 6' x 3' R.C.B. culvert located at Las Posas Road.





KEY  
 □□□□□□◀□□□□□ EXISTING STORM DRAIN  
 ■■■■■■◀■■■■■■■ PROPOSED STORM DRAIN  
 ◀ DRAINAGE PATTERN

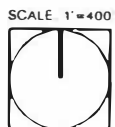


FIGURE 9 Proposed Storm Drain Plan 59



Currently, the subject property is not affected by off-site drainage. Runoff anticipated from the site after development is estimated to be 130 cfs. Runoff will continue to be directed to the southwest corner of the site for discharge to the Las Posas Road ditch.

Capacity of the Las Posas Road ditch is the controlling factor in site drainage. Ditch capacity has been estimated at approximately 77 cfs. The Koll Leonard subdivision is currently discharging a controlled 45 cfs from its retention facility. Therefore, discharge from this site is limited to approximately 32 cfs. This represents an approximate proportional reduction in discharge currently being accomplished by the Koll Leonard retention facility. Therefore, a retention basin shall be incorporated into the Outdoor Recreation area (Planning Area III) located in the southwest corner of the site. This will control the discharge into the existing 6' x 3' double RCB culvert and limit flows to the capacity of the roadside ditch (RCB capacity is greater than ditch capacity).

## 6.0

### WATER

Water service is conveyed to the area from lines directly east and north of the property. Water is supplied by local wells and the Metropolitan Water District. While local pumping is expected to be reduced in the near future, M.W.D. water allocations are adequate.







The City of Camarillo commissioned a "Master Plan for Western Camarillo Water Service" (draft) in 1985. This study has been the basis for water system expansion within the western Camarillo development area. Based on this study, water storage for the City is adequate and supply sources for this region are adequate. The adjacent Koll Leonard subdivision has been developed in conformance to this plan. The TMI property will be required to extend the Koll Leonard system westerly to Las Posas Road. The master plan requires two parallel 12" water mains running east-west, joined by a 12" main in Las Posas Road. Additional north-south connections would tie the 12" parallel mains at cross streets.

The area will have to be annexed to the Calleguas Municipal Water District and Metropolitan Water District, purveyors of imported municipal water. In addition, the property will need to be detached from the Pleasant Valley County Water District since they are purveyors of agricultural water only.

The system will be fully compatible with the master plan concept. Interconnection to the remainder of the City water system will be available from two directions, providing both adequate supply and reliability. The City's Master Plan utilized 3,000 gpm fire flows in its computer modeling, with a land use of CPD. Should fire demand requirements be increased beyond the studied flow, the distribution system will still be adequate. Figure 10 depicts a schematic diagram of the proposed water system.

## 7.0

### SEWER

Sewer service is currently available on site. A 21" trunk sewer (Las Posas Trunk Sewer) exists in the Las Posas Road flowing by gravity southerly joining Pump Station No. 3 located just easterly of Las Posas Road along Pleasant Valley Road. An existing 15" gravity sewer exists along the southerly boundary of the site flowing west to join the 21" trunk sewer. This line serves the Koll Leonard site to the east. Additionally, a 15" sewer exists at the approximate center of the property, crossing under the Camarillo Hills Drain approximately at the juncture with proposed Ventura Boulevard (re-aligned), joining the Las Posas Trunk Sewer.

The City commissioned a "Master Plan for Western Camarillo Sewer Service" (draft) in 1985, which outlined needed improvements to the Camarillo Sanitary District facilities to serve development in this area, concurrent with approvals of development within the service area. The City implemented a funding program to provide the needed facilities. This funding program, the "Mello Roos Community Facilities District" was approved and adopted by the City Council on June of 1989. Through this program, proportionate costs for needed facilities are assigned to benefitting properties and bonds are sold to fund these facilities, repayment of which is accomplished through assessment of the benefitting properties.

Construction of needed facilities will be phased to meet the needs of developing areas.

Additional facilities will be needed to accept increased sewage flows from this and other developing properties in the service area. The City has awarded a contract for the design of new wastewater treatment plant facilities. Scheduled completion of all these facilities is October 1994.

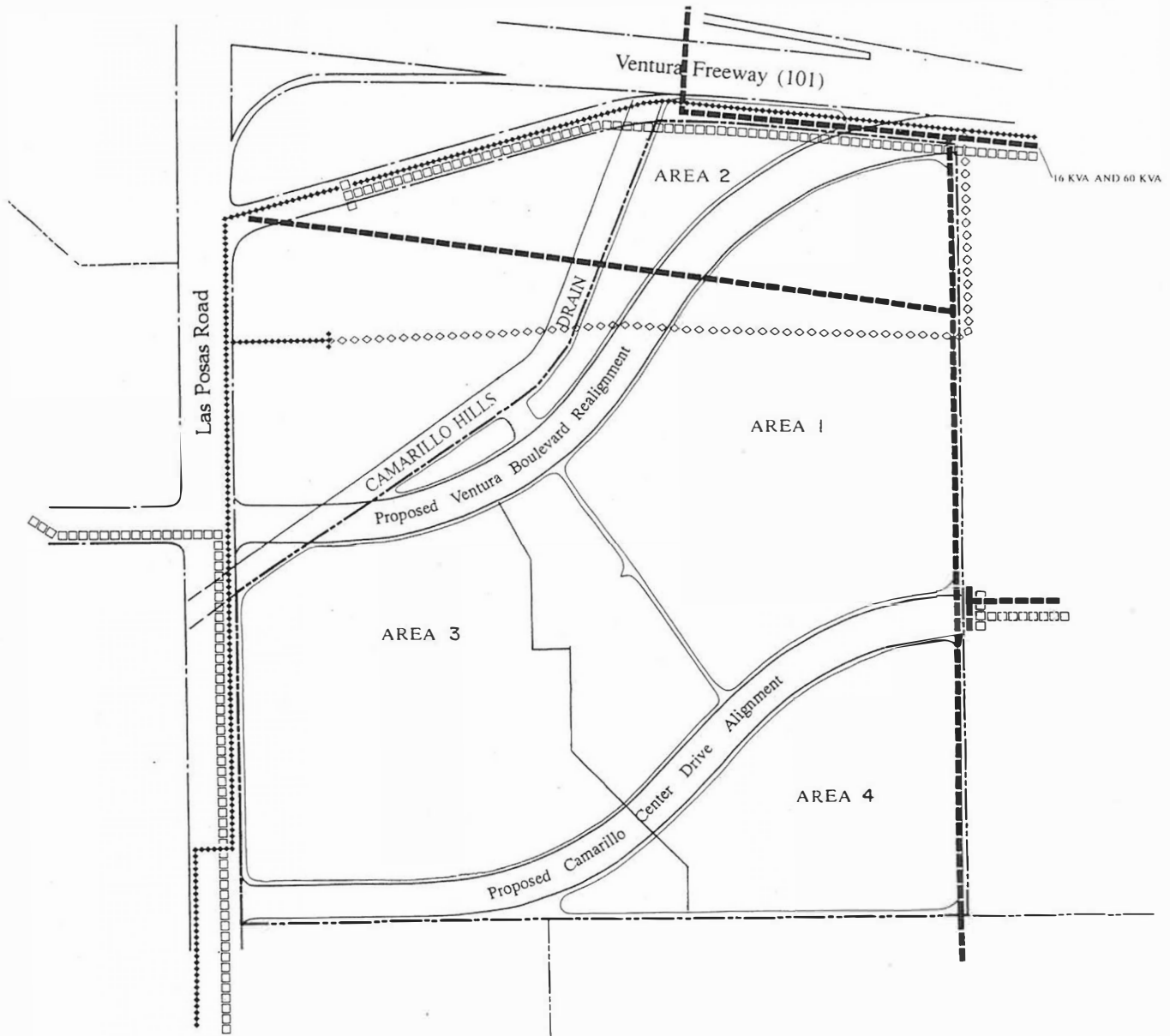
Through implementation of facilities funded by the Community Facilities District, downstream capacity to serve the subject site will be available. The funding mechanism is currently in place. Total system capacity cannot be determined until all facilities are completed.

#### **General Plan Consistency**

The General Plan contains no element pertaining directly to sewage systems. However, the sewage system will be designed and constructed in conformance with local standards. The intent is to construct a reliable sewage system designed to withstand a 100 year flood or an earthquake event without interruption of service.

The subject site will require an extension of existing collection system facilities. Capacity is adequate in the two existing 15" sewers available to serve the site. An anticipated collection system layout is shown on the Water/Sewage exhibit.





- KEY
- 2" GAS LINES
  - 16 KVA ELECTRIC LINES (ABOVE GROUND)
  - ..... SUB STRUCTURE TELEPHONE LINES
  - ◇◇◇◇◇◇◇◇ ABOVE GROUND TELEPHONE LINES

SCALE: 1"=400'



FIGURE 11 Existing Dry Utilities 65



## 8.0

### UTILITIES

Utilities in the context of the Prado de Las Posas Specific Plan include gas, electric and telephone services. Southern California Gas Company provides gas service; Southern California Edison provides electrical service; and General Telephone provides telephone service to the site.

#### Existing Utilities

Three 2-inch gas lines currently serve the site (refer to Figure 11). One line follows along the alignment of Ventura Boulevard. A second line is in the Las Posas Road alignment. A third line on the Koll Leonard property terminates at the east property boundary in Camarillo Center Drive.

All electric lines serving the site are above ground (refer to the Existing Dry Utility Plan). Parallel 16 kilo-volt (kv) and 60 kv lines parallel the northern property boundary. The 16 kv line follows the east property line, heading south to serve for the Koll Leonard storm retention basin pump station.

Approximately 300 feet south of the northeast corner of the site, the 16 kv line along the east boundary splits due west crossing the site. A second 16 kv line terminates at the east property boundary of the Koll Leonard property in the Camarillo Center Drive alignment.

Telephone lines currently serving the site are underground and above ground. Underground lines follow the Las

Posas Road alignment, turning east following the existing alignment of Ventura Boulevard. An underground line branches off the Las Posas line, becoming an above ground line crossing the northern third of the site and heading north on the east property boundary.

#### **Proposed Utility Plan**

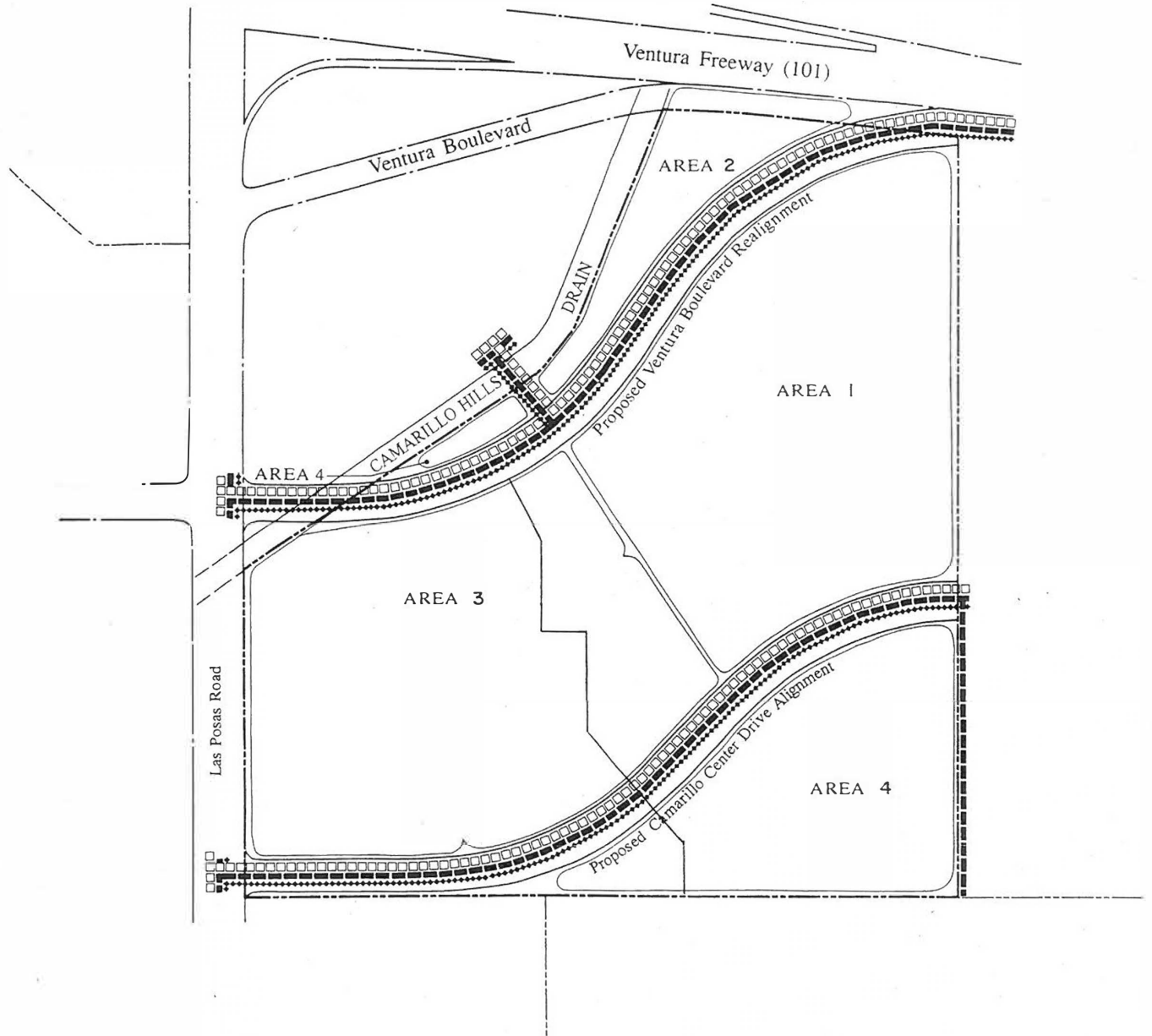
Proposed utilities shall follow the realignment of Ventura Boulevard and Camarillo Center Drive. All utilities shall be underground, with the exception of the 60 kv line. Utility connections shall be made off the main lines in Ventura Boulevard and Camarillo Center Drive.

Abandonment of utility lines penetrating the northern third of the property and along the east boundary between Ventura Boulevard and Camarillo Center Drive will be required. The segment of electric line on the east property boundary between Camarillo Center Drive and the Koll Leonard retention pump station shall remain. This line will be fed from the electric line in Camarillo Center Drive.

The Community Design Element of the General Plan promotes undergrounding of utilities (existing and new) where possible and screening of mechanical equipment and utility cabinets. In compliance with the Community Element, utility lines shall be underground and mechanical equipment and utility cabinets shall be screened from street views.

- Introduction • 1.0**
- Building Height Limitations • 2.0**
- Permitted Building Coverage • 3.0**
- Minimum Parcel Size • 4.0**
- Building Setbacks • 5.0**
- Parking Requirements • 6.0**





- KEY
- □ □ □ □ □ □ □ 2" GAS LINES
  - - - - - 16 KVA ELECTRIC LINES (UNDERGROUND)
  - ..... TELEPHONE LINES (UNDERGROUND)

SCALE: 1"=400'



FIGURE 12 Proposed Dry Utilities



## 1.0

### INTRODUCTION

Each site within the Prado de Las Posas Specific Plan shall be developed in conformance with the following Development Regulations. The Development Regulations supplement the development standards specified in the City's Zoning Ordinance. However, all performance standards specified in the Zoning Ordinance must be complied with (such as equipment and ramping for the handicapped) for the zones and uses proposed by this specific plan. In the event a Development Regulation conflicts with the Zoning Ordinance, the more restrictive shall prevail.

These regulations are to be followed in conjunction with the Land Use Plan exhibit. Together, they set forth the restrictions, specifications and general location for each type of use proposed. Taken together, the Development Regulations section and the Design Guidelines section which follows are intended to provide the framework for design. Innovation is encouraged.

The exhibits in this section establish the parameters within which all development shall occur. Site specific standards and requirements to which individual projects must conform shall be determined at the planned development permit stage based upon the requirements of this section, City policies and ordinances. Additional concepts which must also be addressed in every planned development project, but for which there is some discretion, are set forth in the **Design Guidelines** section.



# AIRPORT HEIGHT RESTRICTIONS

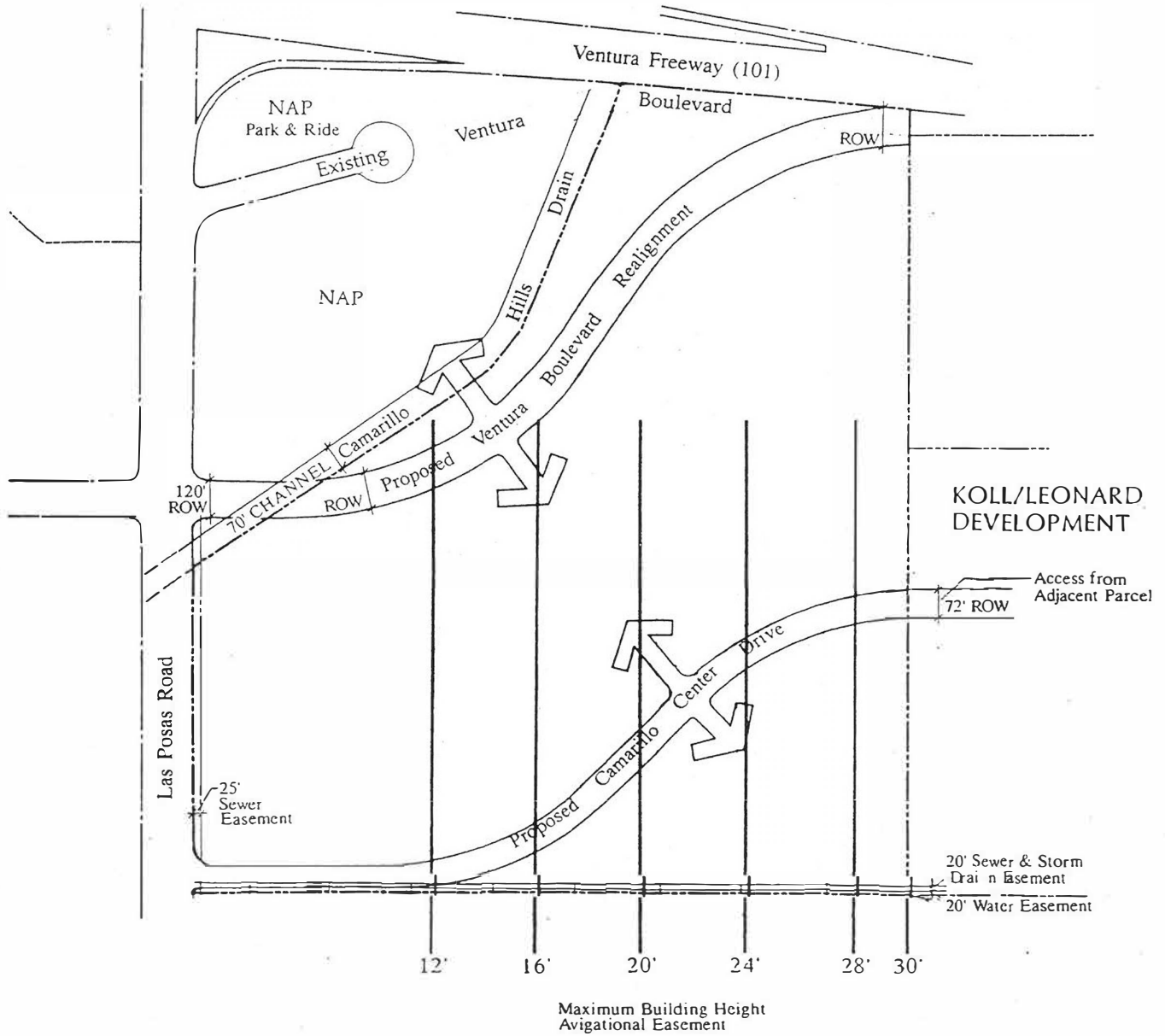


FIGURE 13 HEIGHT RESTRICTIONS



## 2.0

### BUILDING HEIGHT LIMITATIONS

Existing avigational easement rights originally obtained for the Air Force Base limit the maximum building height under the Camarillo Airport approach and take-off pattern. See Figure 13. Height limits vary from less than 12 feet at the west boundary to 30 feet at the east boundary. The height limit extends from the southern boundary across the majority of the site with the exception of Planning Area II and the northern portion of Planning Area I. Although the present touchdown and takeoff areas are approximately 1 mile west of the project site, the glideslope easement controls the range of feasible land uses. For example, residential uses are not considered appropriate due to noise and flight hazards.

Subject to the avigational height limits within the Prado de Las Posas Specific Plan, buildings shall be limited to a maximum number of floors as set forth in the following table. The avigational height limit cannot be penetrated by buildings, landscape material (eg., trees) or hardscape material (eg., light standards, bus shelters). Building heights in excess of the heights established for each zone by the Zoning Ordinance shall require a conditional use permit (CUP), except in areas where restricted by airport height easements.

Table 4

Maximum Building Heights <sup>1</sup>			
Planning Area	Planned Use	Stories	Feet
I	Village Commercial	1 & 2	28-35
II	Commercial (or 4 story Hotel)	1, 2, 3 & 4	28-45
III	Outdoor Recreation Parking	1	up to 19
IV	Office/R&D	1 & 2	20-28

<sup>1</sup>. A Conditional Use Permit is required for any structure over 35 feet.

### 3.0

#### PERMITTED BUILDING COVERAGE

The Specific Plan sets maximum lot coverage standards to limit the building footprint relative to the net parcel size. These factors are based on requirements established in the Zoning Ordinance for similar uses. Parcel size, shape, parking, landscape requirements, setbacks and building heights will also affect the ultimate lot coverage. Table 5 indicates the general type of permitted use, maximum building coverage and zone on which it is based.

Table 5

Building Coverage		
Land Use	Coverage	Base Zone
Commercial Service Office Hotel Park Restaurant Retail	30%	CPD (Commercial Planned Development)
Research & Development Office	50%	L-M (Limited Manufacturing)

4.0

MINIMUM PARCEL SIZE

Parcels may be created by subdivision to allow for financing, phasing, separation of uses and the separate sale or lease of land.

Based upon the project scale, the types of uses anticipated and development plan concept, the minimum parcel size shall comply with the following table:

Table 6

Minimum Parcel Size	
Zone	Size
CPD (Commercial Planned Development)	1 Acre (smaller parcel sizes allowed where reciprocal access easements have been provided)
P-0 (Professional Office)	1 Acre (Single users i.e., Bank, Real Estate Office) 2.5 Acres (Multiple Use) (smaller parcel sizes allowed where reciprocal access easements have been provided)
L-M (Limited Manufacturing)	1 Acre (Single Firm) 2.5 Acres (Multiple Use)

Parcel shape shall be conducive to development and shall accommodate all required clearances, set backs and parking areas. Common drives along property lines may be utilized to encourage a comprehensive approach to planning within and between development plan areas.

**5.0****BUILDING SETBACKS**

A 30-foot (measured from ultimate row) landscaped setback for parking and structures shall be provided along all property lines adjacent to streets. Common service corridors along the east property boundary and structures adjacent to the Camarillo Hills Drain are excepted. All additional setbacks as required in the Zoning Ordinance shall apply, except that no zero ("0") setback shall be used where landscaping is required along the site perimeter. Zero setbacks on side property lines may only be permitted for lots where reciprocal access easements are provided to the adjoining parcel and where additional landscaping has been provided and approved under site plan review. Greater setbacks shall be provided where buildings exceed two stories. The added setback shall equal the amount of added building height.

Buildings in Planning Area IV shall be set back at least 75 feet from the southern property boundary in order to create a buffer which may be used for walkways, parking and landscaping. A 20-foot wide densely-planted area shall be provided to serve as a screen to separate the agricultural activity from the development area.

**6.0**

**PARKING REQUIREMENTS**

On-site parking shall be provided in accordance with Chapter 19.44 of the Camarillo Municipal Code. The amount of parking, design of parking areas and parking space dimensions shall be provided as set forth in the Off-Street Parking Ordinance.

Each use shall provide its required parking area. Off-street parking areas shall not be located more than one hundred and fifty feet from the site it is intended to serve. Where off-street parking is located on a different site than that of the building it is intended to serve, a shared parking agreement to the satisfaction of the City must be recorded.

Vehicular and pedestrian linkages must be provided to create a unifying element and promote comprehensive planning between uses. These interconnections will aid traffic circulation and promote interactions between different uses.

Table 7

Land Use	Required Parking
Commercial Services	1 space/250 square feet
Hotel	1 space/room plus 1/employee
Restaurant	1 space/3 seats plus 1 space/100 s.f. of kitchen area; minimum 10
Retail	1 space/250 square feet leasable space
Offices	1 space/250 square feet
Research and Development	1 space/500 square feet (20% office allowed)

Uses not specified in the table shall be subject to criteria established in the Off-Street Parking Ordinance. No on-street parking shall be allowed on Ventura Boulevard, Las Posas Road or Camarillo Center Drive.



**General Guidelines ● 1.0**

**Building Design ● 2.0**

**Site Planning ● 3.0**

**Signage ● 4.0**

**Landscape ● 5.0**



## 1.0

### GENERAL GUIDELINES

#### 1.1

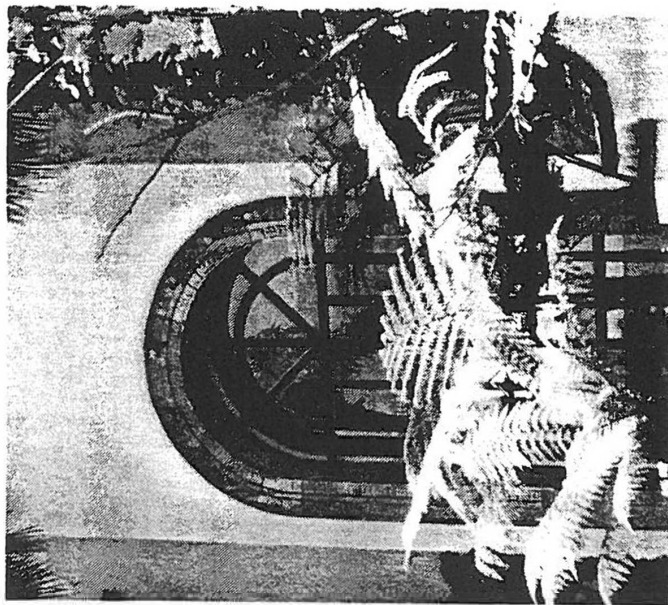
##### INTRODUCTION

This section of the Prado de Las Posas Specific Plan sets forth specific design standards which are expected to help to create the character for the multi-use center adding a dimension beyond the mere physical placement of buildings, parking areas and streets. The design guidelines are consistent with the objectives of the Community Design Element of the City's General Plan. This will help to ensure that the design goals are consistent with the project's objective: to create a high quality center designed as a multi-use development integrating retail, support commercial, office, open space and research and development (R&D) uses.

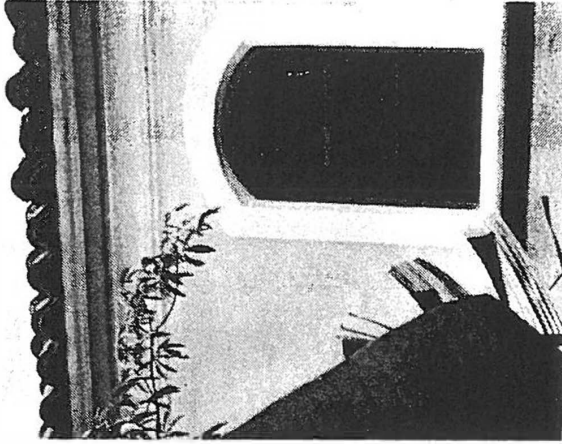
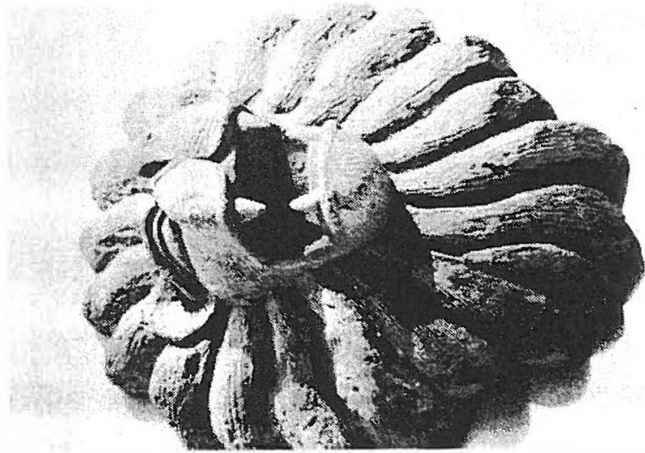
In addition, these guidelines seek to reinforce and strengthen the design criteria specified under the Koll Leonard Specific Plan by providing standards which will result in a development consistent and compatible with the adjoining specific plan area.

The Prado de Las Posas site is located within the freeway corridor through the City of Camarillo. Developments within the corridor as can be clearly evidenced, exhibit a Spanish or Mediterranean design theme. This is a result of the Heritage Zone standards contained in the Community Design Element of the General Plan.

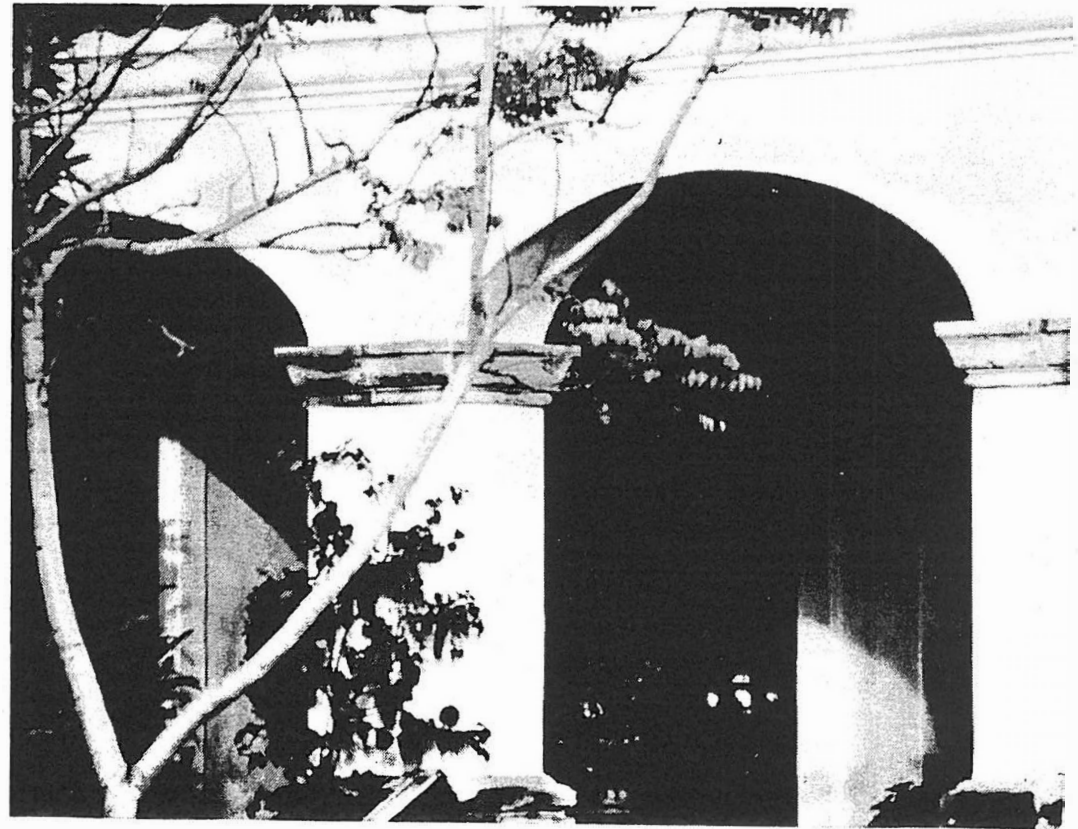
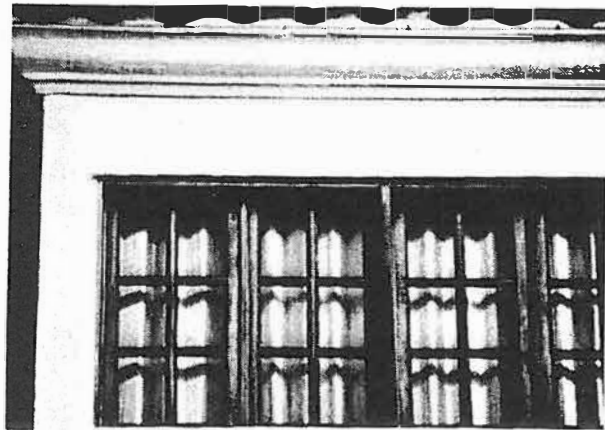
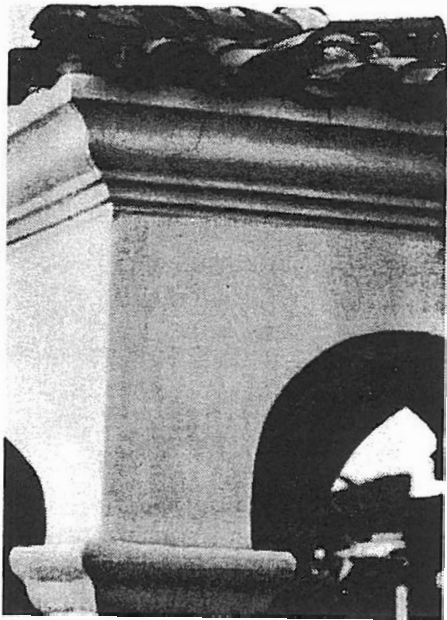




*Imagery:  
Coping, landscape, roof tiles, windows, archivolts trim*







*Imagery:  
Coping, archways, columns, windows, light and shadow, landscaping*



The proposed development as discussed in this section will retain this architectural style within the parameters of the Community Design Element, yet will still be able to create an identity of its own within the community.

## 1.2

### GOVERNING DOCUMENTS

Prado de Las Posas is governed by Camarillo's General Plan, Zoning Ordinance, and building and subdivision codes. Applicants should refer to current City Ordinances for compliance.

## 2.0

### BUILDING DESIGN

## 2.1

### ARCHITECTURAL STYLE

The design concept for Prado de Las Posas envisions a development reminiscent of a Spanish or Mediterranean Village. Buildings may be designed having a traditional style or "period" style such as buildings having a Mission, Monterey, Santa Barbara or Early California influence. Most of these styles incorporate some common architectural treatments and elements which help make up the basis for the Prado de Las Posas design guidelines.

The design of buildings, particularly those within the R&D area, may also present a more contemporary style through incorporation of certain elements characteristic of the

"period" styles. Regardless of the approach, the buildings shall be designed to create a warm, enriching atmosphere that will blend with other developments in the City and complement the character of the adjoining Koll Leonard Specific Plan architecture.

All buildings shall be designed to achieve a balance between windows and wall mass, and between buildings and open spaces. Shifts in walls and massing can add balance, design interest and help to create planting areas, entries or plazas at the ground level.

Building design shall include the use of towers, balconies, arches, colonnades, chimneys, trellises, corbels or other types of elements as a means of balancing volumes and creating interest to the viewer. Each elevation shall have a well-designed appearance blending well with the other elevations. No stagefront treatment shall be acceptable.

## 2.2

### **MATERIALS PALETTE**

The selection of building materials shall enhance the central architectural theme and shall be used in a manner to promote cohesiveness within the development. The prominent color themes shall be of an earthtone or light hue using off-white with accent materials and compatible colors to create visual interest. Accent colors shall be limited to suitable locations such as wood trim, window/door frames, score lines and glazed tiles. The material finishes and color ranges for the Prado de Las Posas

Specific Plan shall be as shown in Table 8. Alternative materials can be approved by the City as part of the review process. Metal finishes will be limited to such items as hardware, flashing, vents and doors. Exposed metal shall be painted to match the adjacent surface or with an appropriate accent color.

**Table 8**

<b>TYPE</b>	<b>MATERIAL</b>	<b>COLOR</b>
Roof, sloping	Clay barrel type	Red, flashed, brown
Roof, flat	Crushed rock	Color to match tile
Walls	Stucco	Off-white or beige, "float" finish
	Concrete	Textured, stucco-wash, or painted
Masonry	Block	Slumpstone or Padre brick
Accents	Tile	Glazed or unglazed
Wood	Roughsawn milled	Dark brown stain painted, or accent color
Pavement accents	Tiles, stamped concrete or textured	Integral color
Fences and gates	Wrought iron	Painted black, green, blue or reddish brown

### 2.3

#### ROOF DESIGN

Multi-planed sloping tile roofs of gable and hip designs are appropriate for the architectural style being proposed. However, flat roofs which are visible from roadways, taller buildings or balconies are to be avoided. In some cases for large retail or other uses, this may not be possible and may require other design elements, such as the use of parapets or other elements. Roof tiles indigenous to the architectural style are desirable with red, flashed and brown colors being acceptable. Actual colors will be subject to Planning & Community Development Department review. If a flat roof is to be utilized, it should match the color of the tiles. Additional articulation should include the use of simple roof overhangs to create shadows and contrast.

If skylights are to be utilized, they should be integrated into the roof design and should be flush with the roof and not appear to be "added on." Skylight glazing shall be clear or solar bronze. White glazing is prohibited. Skylight framing materials should be dark bronze anodized or must be painted to match or blend with the roof.

### 2.4

#### WINDOWS

Windows are typically rectangular in shape or may feature arched or round-headed openings. Desirable features include multi-paned windows which have several small lites in each window rather than one large lite of glass.

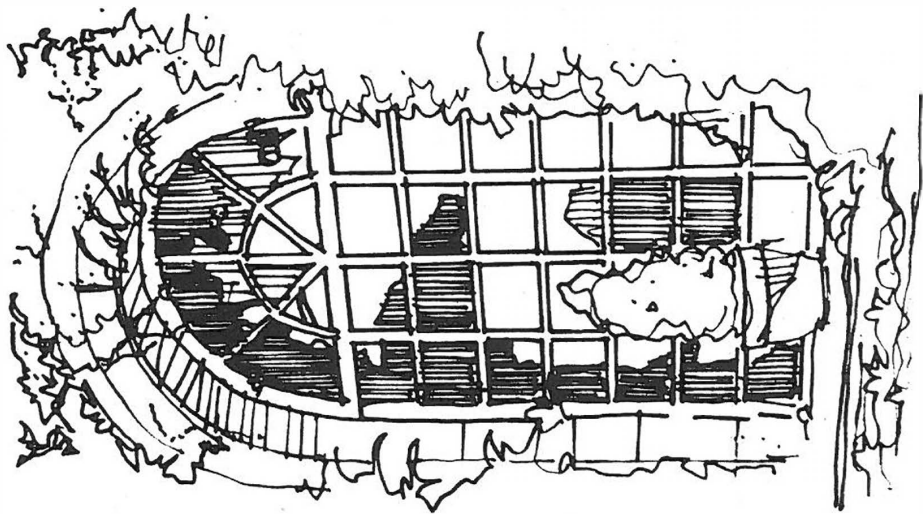
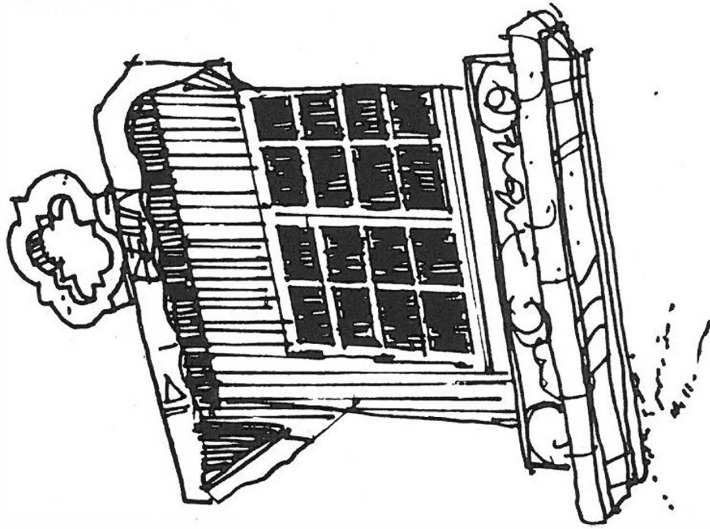
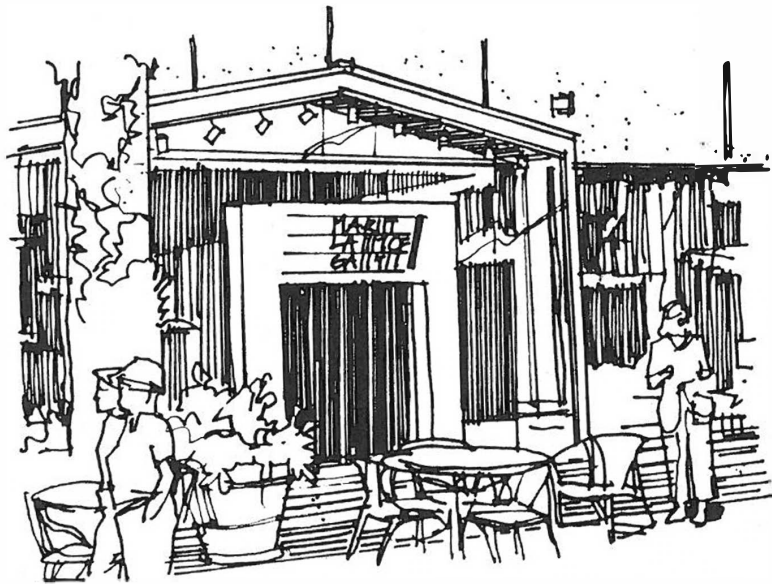


FIGURE 16 Windows







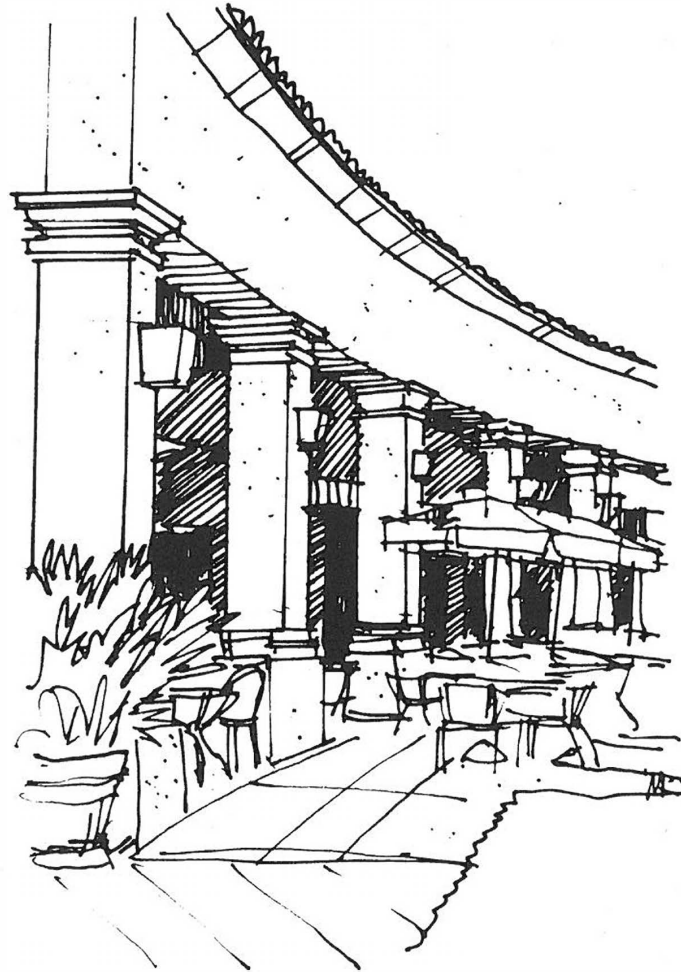






FIGURE 19 Typical Village Commercial Facades



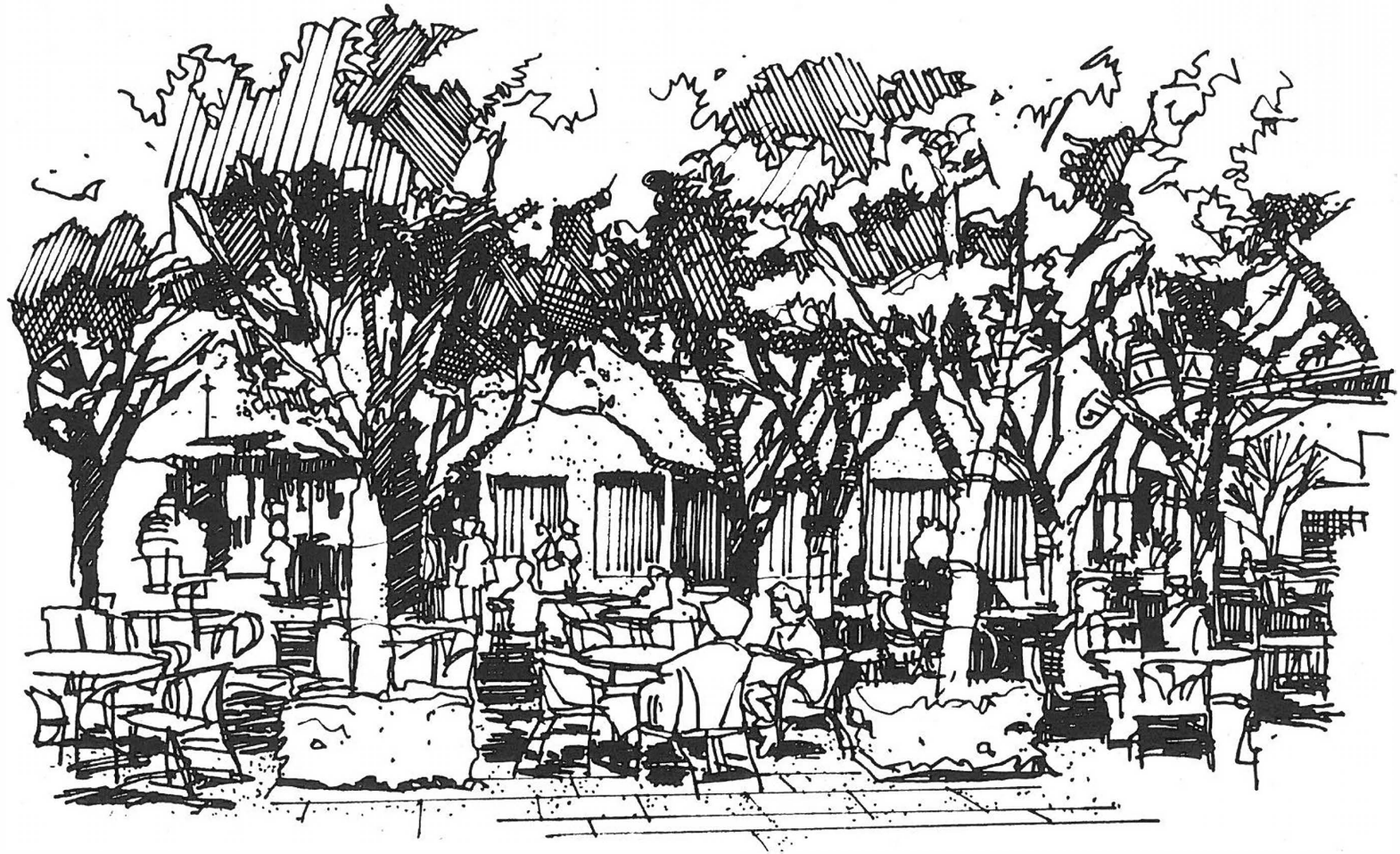


FIGURE 20 Plaza Seating Area



Recessed windows are an important characteristic of the Spanish or Mediterranean style. This can be achieved by designing projecting windows with external plant boxes and exterior sills. Projecting windows may also be used to add articulation to wall surfaces. Silver, gold or mill finish window frames are prohibited while reflective glass and metal awnings are also prohibited.

### 2.5

#### **DOORS**

Doors, like windows, are typically rectangular or round-headed. The doorway should be fully recessed in order to convey the appearance of thick, protective exterior walls.

Commercial and Village Commercial users may utilize contemporary door styles and display windows provided the integrity of the wall color and overall architectural theme is maintained. The intention is to encourage enough diversity in order to promote a lively street scene.

### 2.6

#### **COLUMNS AND ARCHWAYS**

Buildings should incorporate the use of arches or arcades into the overall building design. Columns shall be substantial in appearance being at least 20 inches in cross section.

**2.7****MECHANICAL EQUIPMENT**

All rooftop and ground-mounted mechanical equipment shall be shown on the plans for review during the planned development permit consideration. Rooftop equipment must be mounted so that the top of the units are completely screened by the building parapet. As an alternate, rooftop equipment may be concentrated at locations and provided with an architecturally designed screen wall where equipment cannot be screened by the height of the parapet. Rooftop mechanical screens shall be designed using a ribbed metal siding placed on a frame of varying angles depending on the design of the building. The screen shall be dark bronze in color or shall be of a color to match or accent the building. The upper edge of the metal shall be trimmed with a finishing strip. Should rooftop equipment be visible from adjacent taller structures, it shall be concentrated in an area so as to minimize views and shall be painted a color to match that of the roof. Ground-mounted equipment shall be screened by walls integrated into the site design. Landscape screening may be permitted for ground-mounted transformers where large planting areas will provide sufficient screening.

Gas and electrical equipment must be screened from view and integrated into the building design.

### 3.0

#### SITE PLANNING

Site design involves a variety of factors such as building setbacks, parking design, landscape design, lot coverage, viewscales and other considerations. The design should result in a functional relationship between the various elements in addition to establishing aesthetic qualities and promoting key design concepts.

Key features of the site design will be the creation of the village concept with pedestrian linkage, plazas, internal drives, activity nodes, meandering walks and a strong landscape program.

### 3.1

#### BUILDINGS

Since much of the specific plan site will be highly visible from either Ventura Boulevard or the Ventura Freeway, maintaining view penetrations from off site is important. Axis orientation of buildings and freestanding structures along both north and south sides of Ventura Boulevard can help maintain view penetrations into the site from Ventura Boulevard and the Ventura Freeway. Siting of buildings shall also allow for vehicular and pedestrian movements from within the specific plan area to the Koll Leonard property without using Ventura Boulevard or Camarillo Center Drive.

An internal driveway through the center of the site will be utilized to create a pedestrian core which will link focal

points on site with features such as special paving, pergolas, sculptures, fountains, or traffic circles. Buildings fronting the pedestrian core shall be oriented toward it with breaks between buildings to direct traffic to larger ground level parking plazas. Some limited parking may be allowed adjacent to buildings fronting the core.

One possible concept of the Village Commercial area is known as the Mediterranean "Neo-Piazza" which integrates building orientation, pedestrian and automotive circulation. Buildings are inwardly oriented, framing courtyards which serve as parking areas or potential gathering areas for seasonal promotional activities such as: fairs, concerts and dining. In this context, the scale and treatment maintains a balance between the vehicle and pedestrian.

The goal of site planning in the R&D Planning Area IV is to develop a relationship between structures that creates a campus-like atmosphere. Buildings may be clustered with parking located in a separate area. Building orientation, form and design features such as landscaping, hardscape, walkways, plazas, entrances and service areas should all be integrated.

Due to the height restriction gradient overlaying the site, structures should be plotted toward the eastern portion of Planning Area IV with parking to the west. Height restrictions in Planning Area IV will allow one-, and possibly two-, story structures.

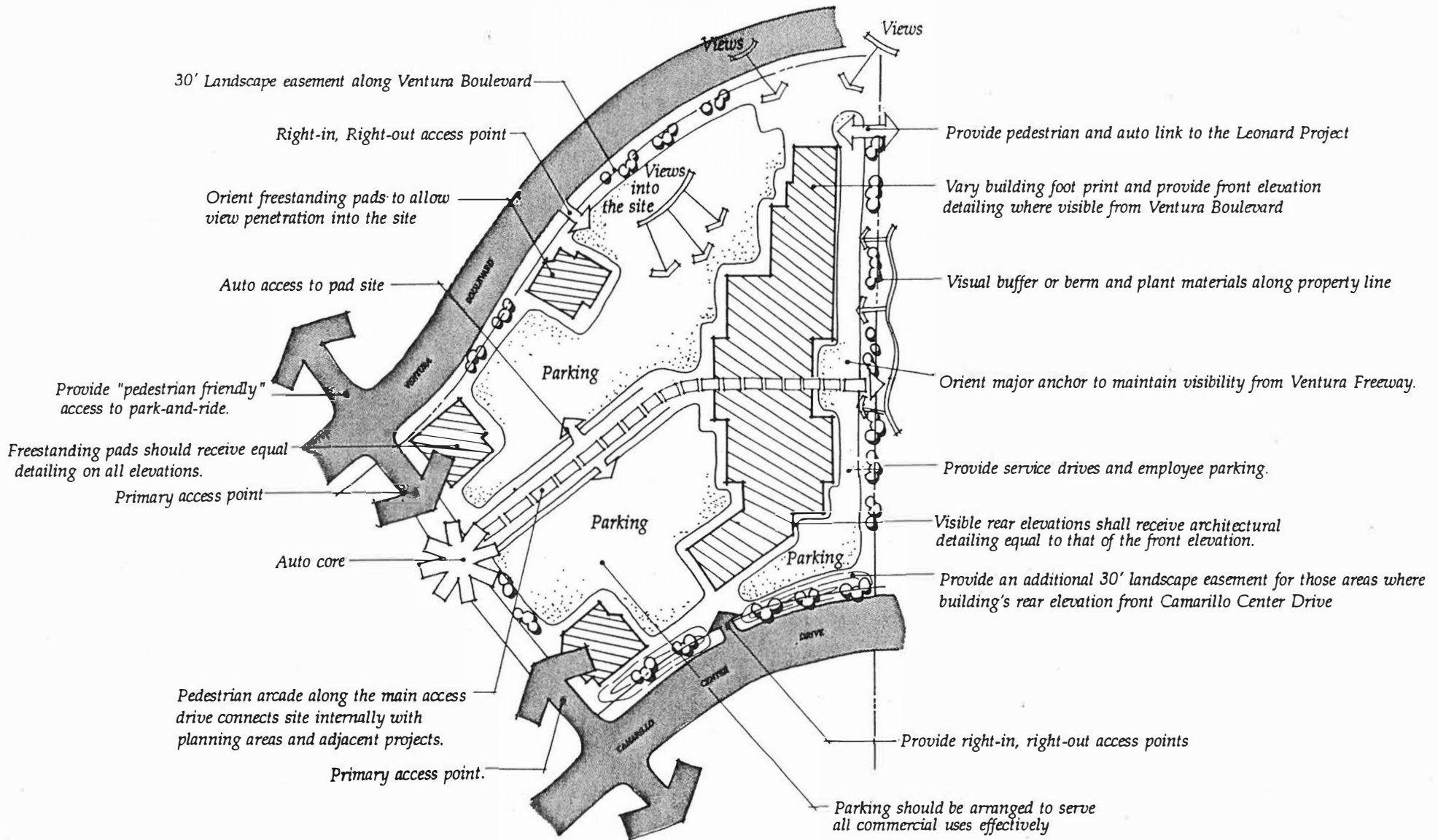


FIGURE 21 Planning Area 1 • Site Planning Concept Commercial



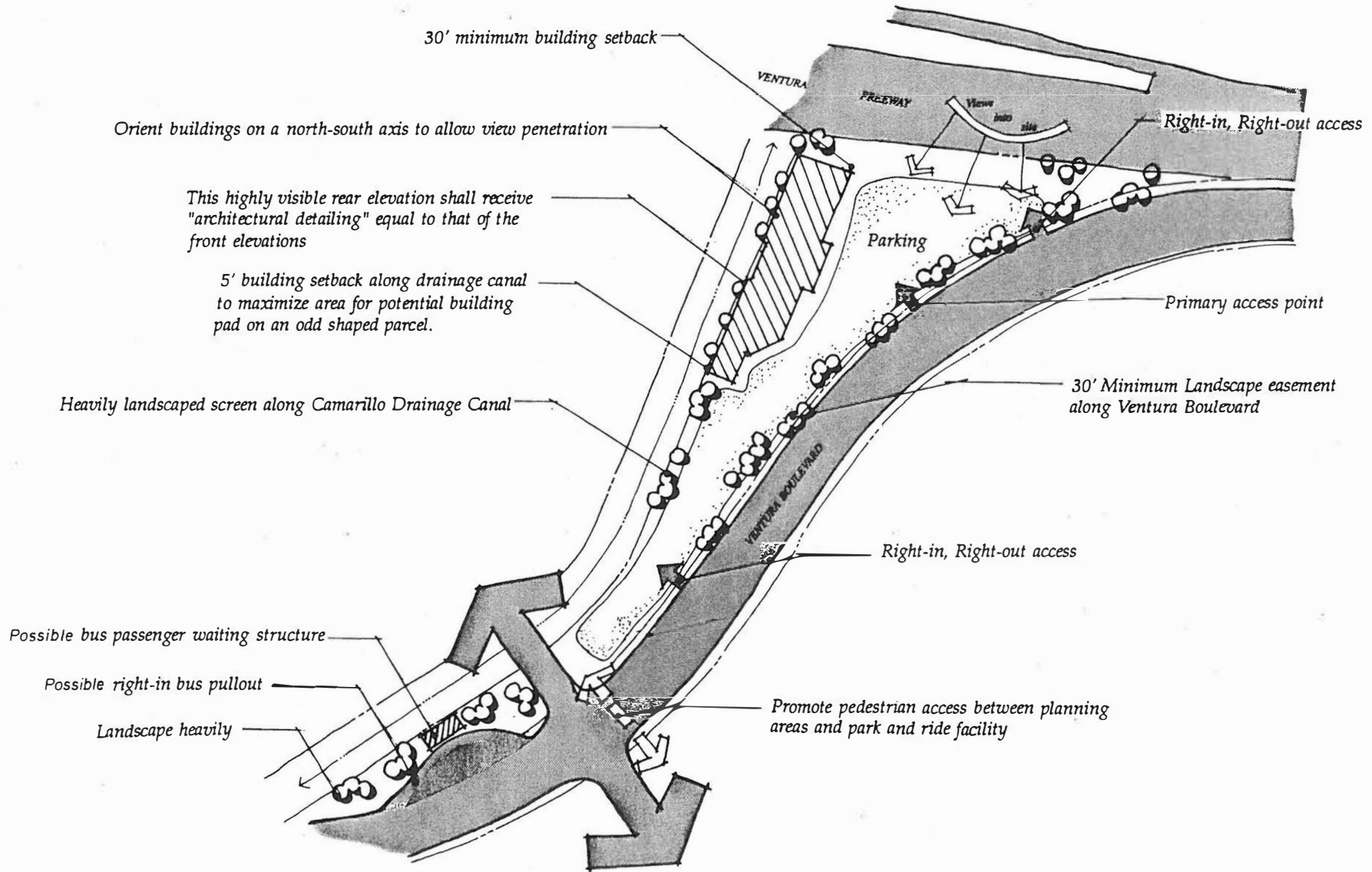


FIGURE 22 Planning Area 11 Site Planning Concept • Commercial



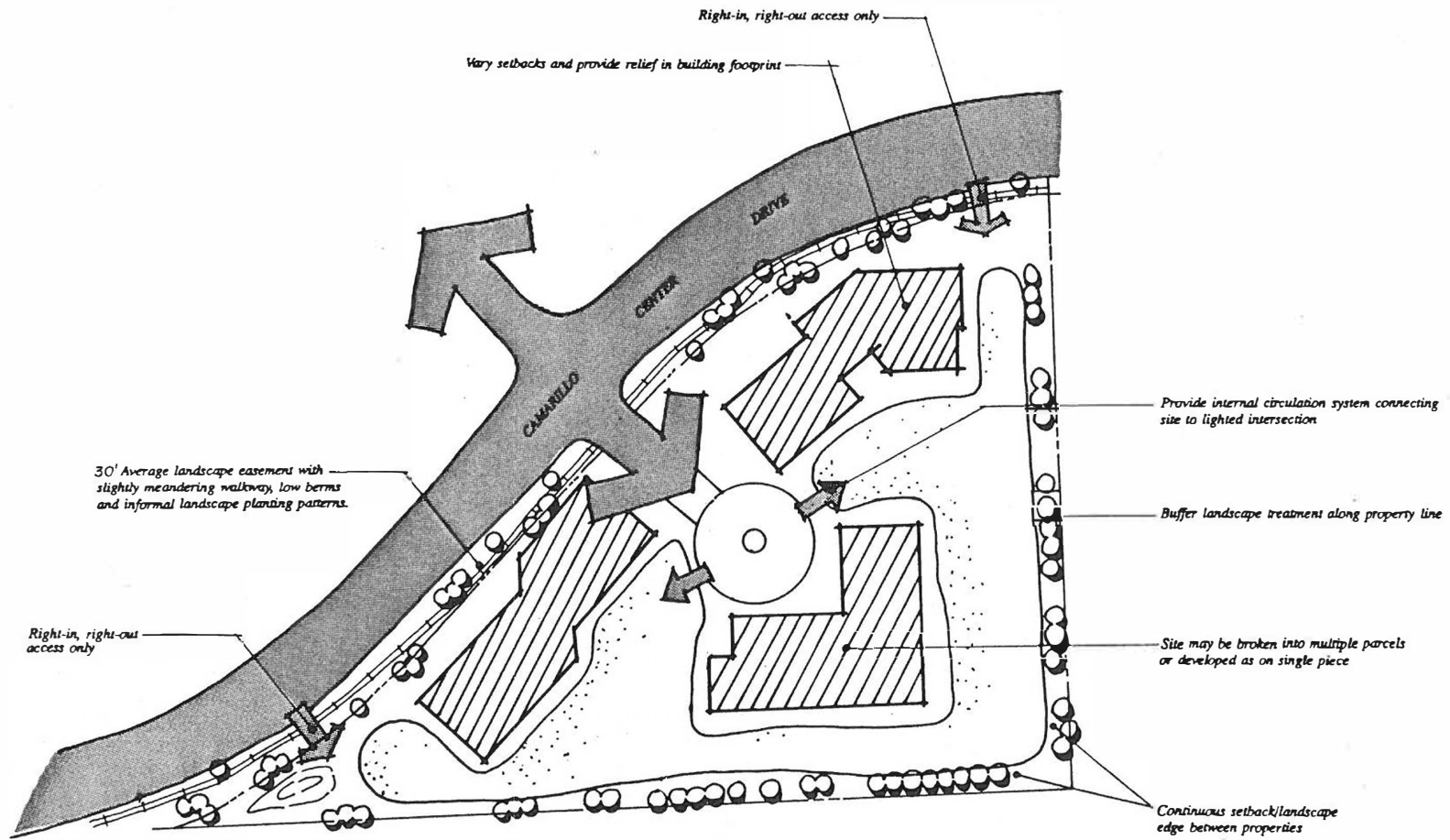


FIGURE 23 Planning Area IV Site Planning Concept Office/R&D



In Planning Area IV, buildings should front Camarillo Center Drive with service areas oriented inward or to the east or south. Building design should recognize the adjacent agricultural activities with the design and location of building openings such as vents, air intakes, and roof-mounted equipment located as far north as feasible in order to mitigate the effects of agricultural activities such as noise, dust, fertilizers and pesticides.

### 3.2

#### **INTERIOR ROADWAYS/PEDESTRIAN SPINE**

Interior roadways should be designed to serve both vehicle and pedestrian traffic. The scale and design elements of these interior roads should avoid conflict between pedestrian and vehicular movement to the greatest extent possible.

In addition to limiting the amount of street parking, the width and number of automobile travel lanes should also be limited so that it does not create a barrier to pedestrian movement. Interior roads should convey traffic to parking plazas and service areas and should be limited to two travel lanes (one in each direction) whenever possible. Lengthy segments of roadway without opportunity to access parking should be avoided as should unbroken segments of interior driveways exceeding 300 feet in length.

Interior roadways should be designed to discourage high speed traffic (fifteen miles per hour or more) and "drive through" trips directly between Ventura Boulevard and

Camarillo Center Drive yet provide opportunities for movement between the TMI and Koll Leonard properties. Design features which discourage these trips may include undulation of the roadway, varying roadway widths, overhanging landscape (restricting distant views), textured pavers, speed bumps, stop signs, traffic circles, parking plazas, and limited parallel or perpendicular on-street parking abutting the driving lanes.

A combination of generous pedestrian walkways and building setback should be integrated with roadway design. Pedestrian walkways should be a minimum of 10 feet in width. Building setbacks along the pedestrian spine(s) may be enhanced in certain areas which may help create pockets for activities such as outdoor restaurant dining areas.

Use of hardscape elements such as pedestrian lighting, rolled curbs, pergolas, special pavers, bollards or trash receptacles may create a symbolic boundary between pedestrian and automobile traffic.

Similarly, individual or clustered landscape elements such as street trees, pockets of low shrubs, groundcover, colorful annuals or turf may be utilized to create this boundary. The intention is to form a boundary not a barrier. Use of walls, fences and continuous hedges is discouraged.

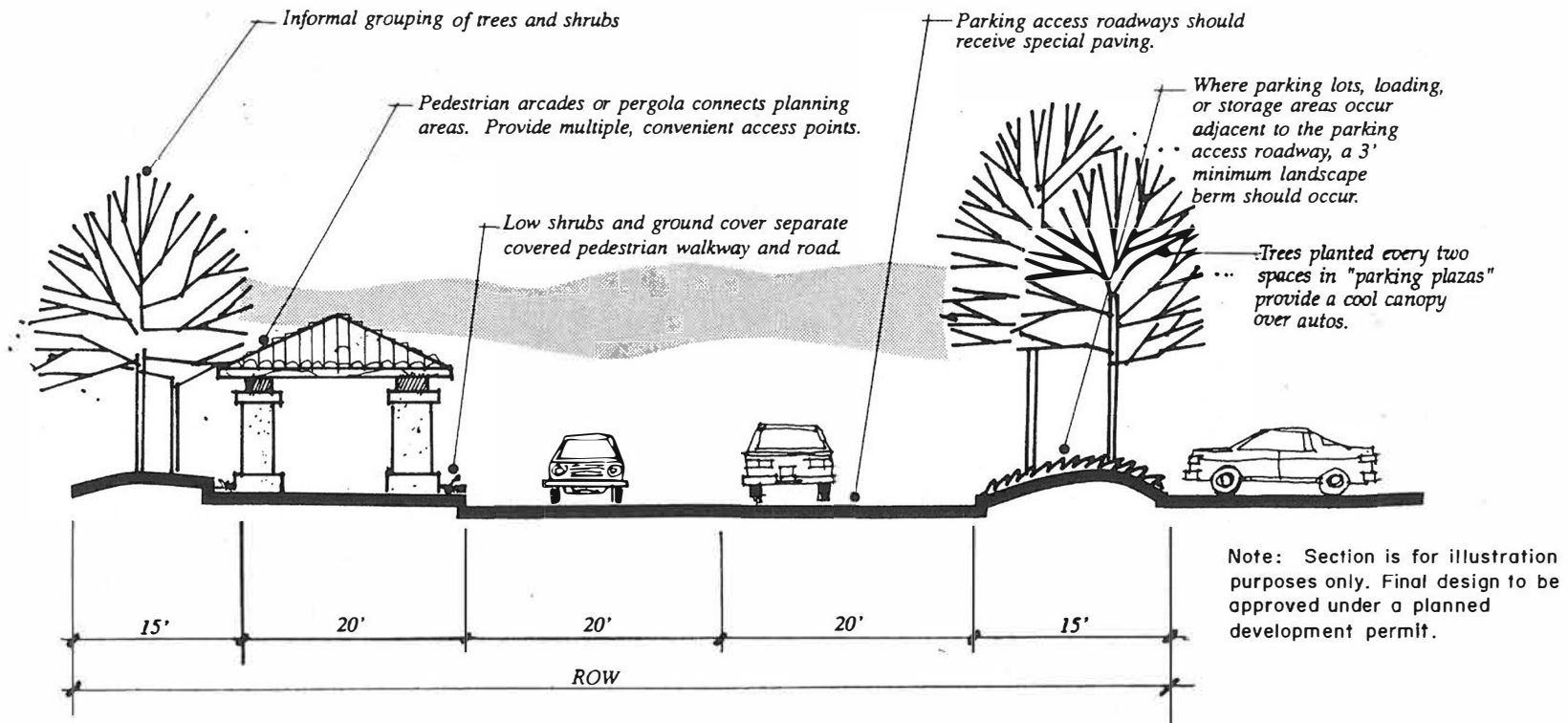


FIGURE 24 Pedestrian Access in Large Parking Areas



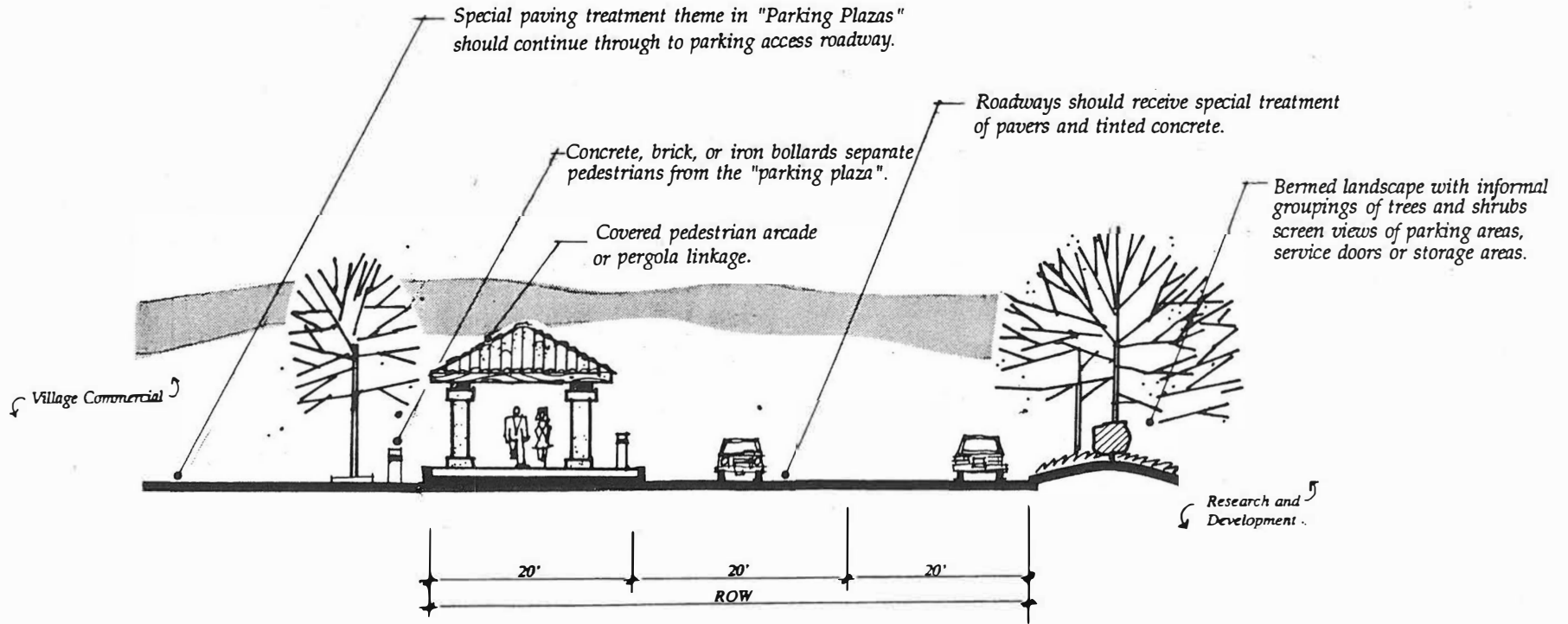


FIGURE 25 Pedestrian Access Between R&D and Village Commercial



### 3.3

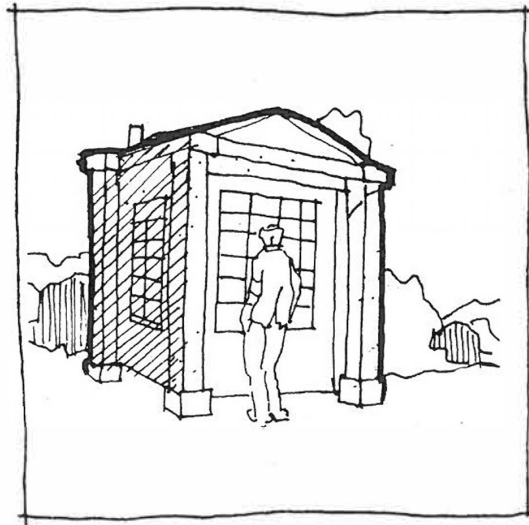
#### **STREET FURNITURE**

Hardscape elements in all of the planning areas shall be of a similar design providing for continuity throughout the specific plan and for compatibility with the adjoining Koll Leonard Specific Plan area. Hardscape elements include, but are not limited to: pedestrian lighting, bollards, benches, trash receptacles, planters, mailboxes, drinking fountains and bike racks. These types of items establish the human scale, and, if coordinated and consistent, help to create a visual and physical connection between buildings, landscape and hardscape.

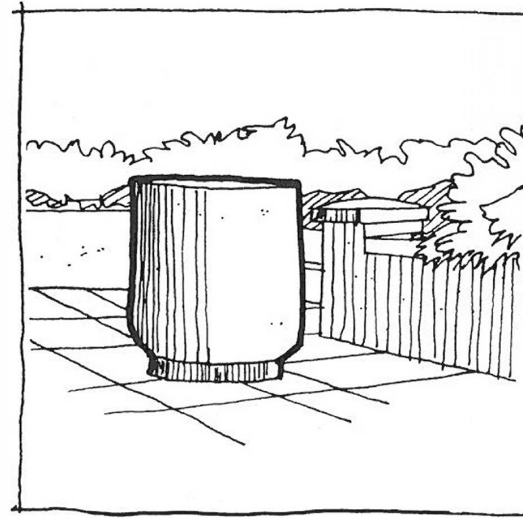
In addition, larger scale elements such as bus shelters, trellis structures, gazebos and the like also act as street furniture and should be coordinated to bring about a sense of a unified design approach. For clarification, the use of street furniture is not limited to areas within the street corridors. Street furniture is meant to be integrated into the overall development through the use of items based on their function and established throughout the project.

Advertising or other types of signage shall not be permitted on street furniture. Street furniture items shall relate to the colors of the buildings, be vandal-resistant and be complementary in terms of materials and textures.

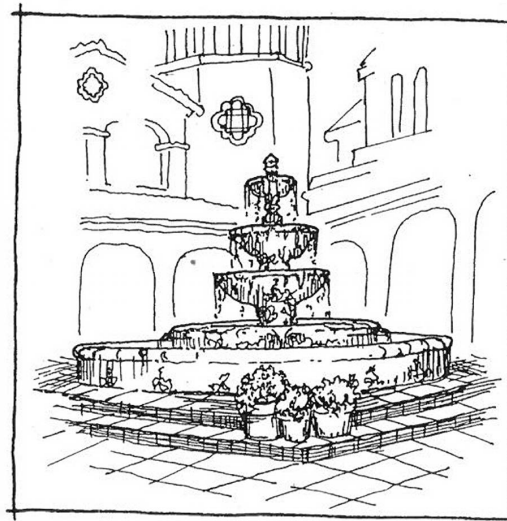




*Kiosk mailbox structure*

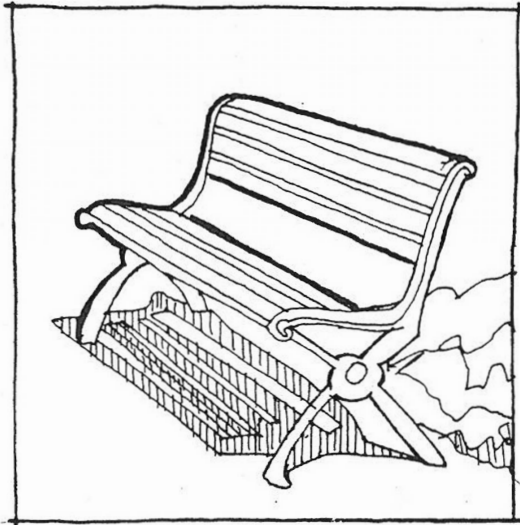


*Ceramic trash enclosure  
with drop-in trash can*

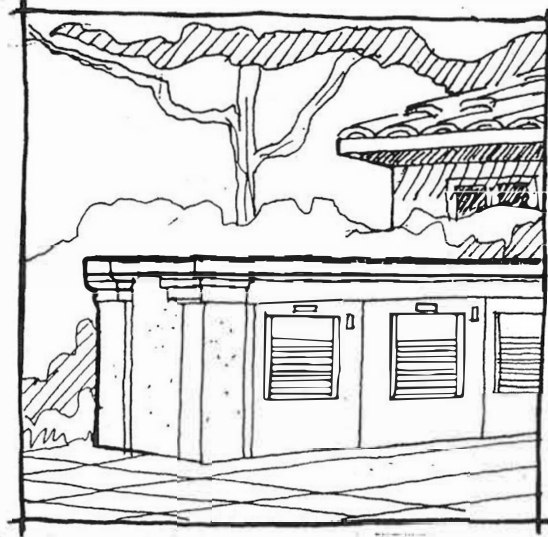


*Pre-cast concrete sculpture  
creates interest*

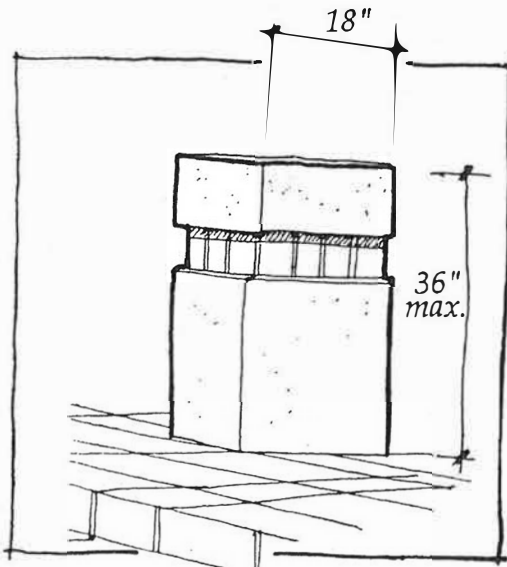




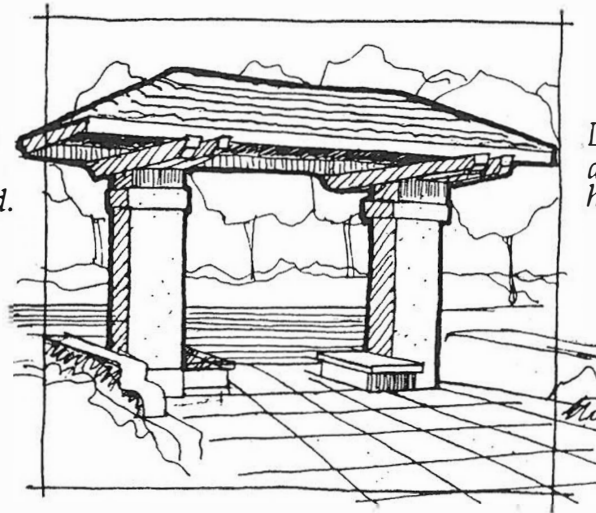
*Benches shall be constructed of wrought iron or masonry*



*Newspaper vending stands shall reflect the architecture of surrounding buildings*



*Bollards separate pedestrians and vehicles. Lighting may be incorporated.*



*Locate bus shelters in areas that have the highest passenger volume.*

FIGURE 27 Hardscape Elements



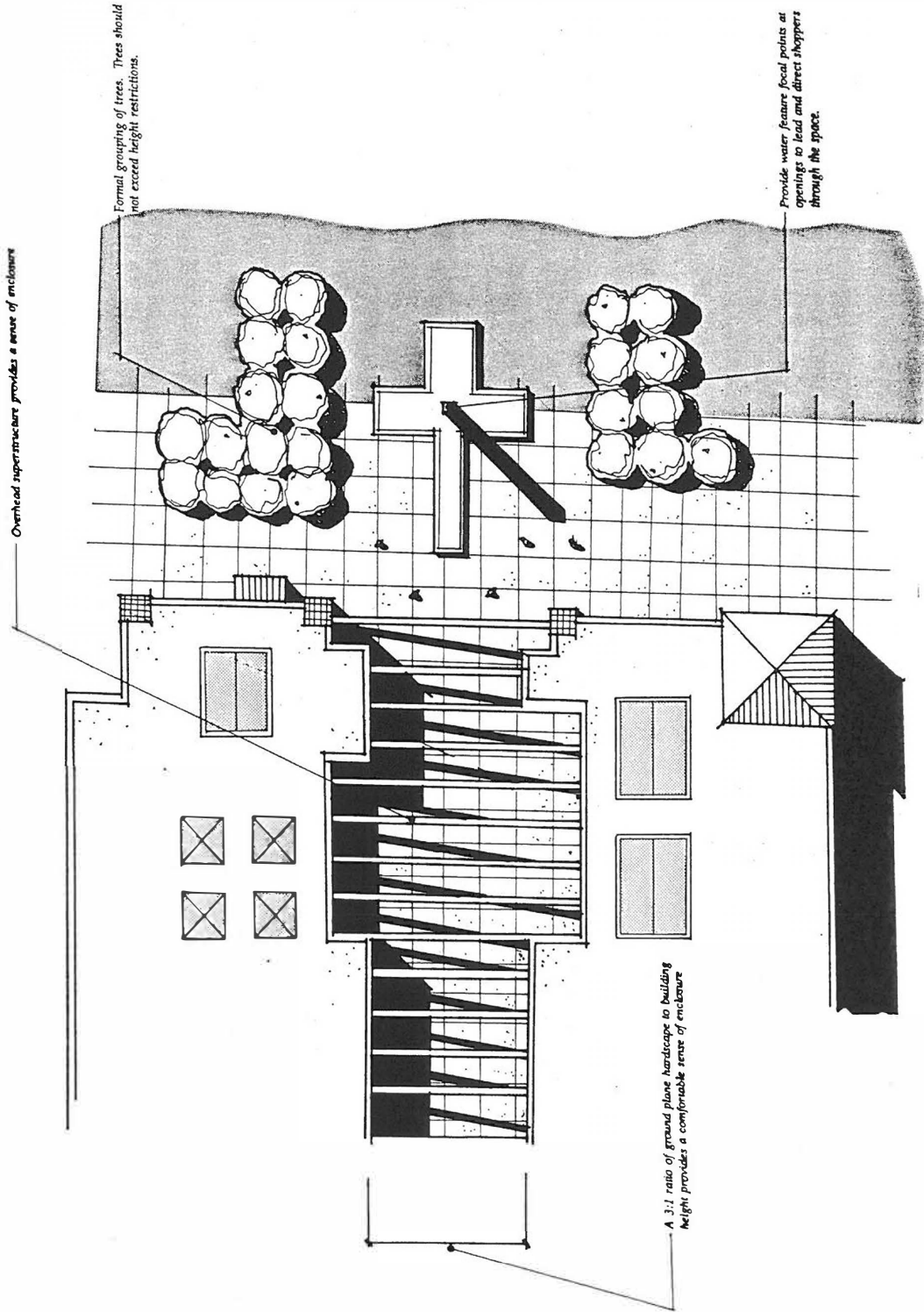


FIGURE 28 Pedestrian Linkage



### 3.4

#### PEDESTRIAN LINKAGES

Pedestrian linkages shall be provided between the various uses. This shall include plazas and arbor-covered tree-lined walkways leading from the pedestrian core. Pavement textures, planting areas, street furniture, pedestrian lighting and ramps shall be incorporated into the design.

### 3.5

#### LIGHTING

Lighting is intended to provide adequate illumination for the safety and comfort of vehicular and pedestrian movement. The hierarchy of illumination for streets, entries and individual structures must be clear of glare, hot spots and inconsistencies. The goal is not to achieve a monotonous lighting level and pattern, but one which responds to the varied land use and street edge conditions found within Prado de Las Posas.

Due to proximity of the Camarillo Airport, light must be shielded and directed downward. Glare must be retained within the boundary of each individual project.

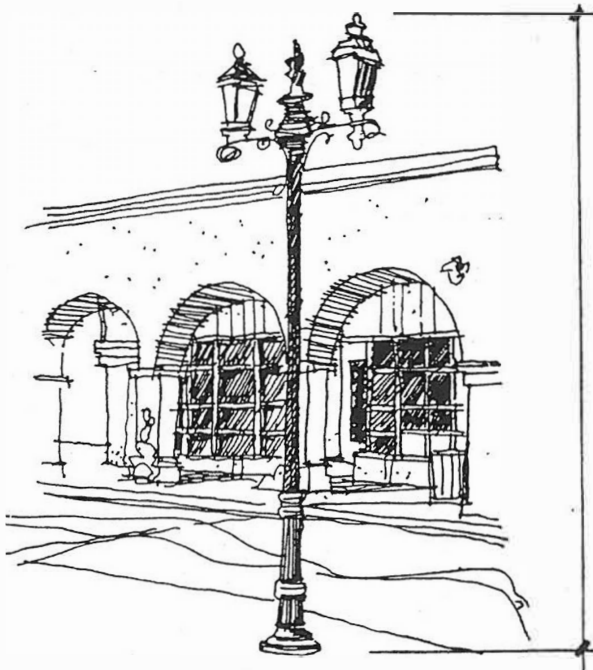
Street lights are to be provided along Ventura Boulevard, Camarillo Center Drive and along other interior streets. Lighting shall utilize the standard City-specified pole and lighting fixtures and shall be provided at a location and spacing per the City Engineering Services Department specifications.

Lighting for parking lots shall be provided through the use of pole-mounted light fixtures a maximum of 16 feet in height. The location and spacing of light fixtures should be adequate to provide for a level of lighting which is safe for both pedestrian movement and security. Wall-mounted fixtures may also be utilized and placed on the building walls so long as the design of both pole and wall-mounted fixtures provide for shielding of the light source on the sides and top of the fixture.

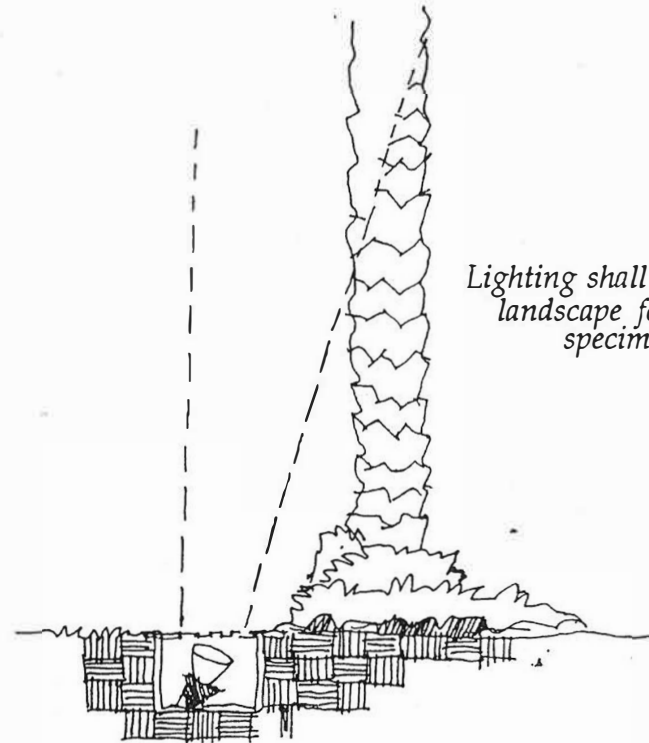
The use of low bollard and ornamental pole-mounted fixtures is envisioned within pedestrian areas, plazas and areas where the avigational easement may restrict taller light sources. Fixtures in these areas should be of a scale that relates to the pedestrian. Area lighting should attempt to illuminate the architectural wall planes and columns to obtain as much of the desired illumination from reflected light as possible.

Architectural lighting should be indirect and from a limited number of sources to create shadows, accent relief and building outlines. Architectural lighting should be used sparingly, not for advertising.

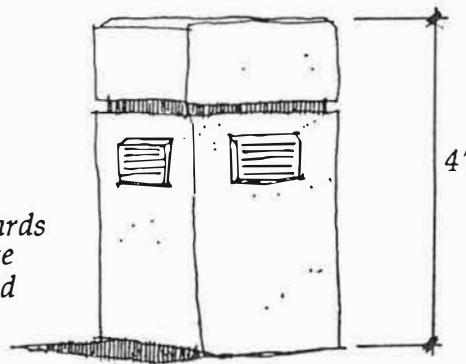
Landscape lighting is permitted so long as the source of light is fully shielded from the view from streets and adjoining properties and that the light is concentrated onto the plant materials.



Light standards shall not exceed 16 feet, warm white light is encouraged. Light is to be directed down to preserve the night sky.



Lighting shall be used to highlight landscape features such as specimen trees.



Concrete or metal light bollards may be used to accentuate building entrance areas and provide ground lighting.

FIGURE 29 Lighting Guidelines



An overall lighting program would be required as part of the planned development package. A program utilizing similar light fixtures, spacing and type of light would be required throughout Prado de Las Posas and should complement existing light sources within the Koll Leonard Specific Plan. The use of low-intensity, energy-efficient light sources is encouraged.

### 3.5

#### **TRASH ENCLOSURE**

All developments within the specific plan area shall be required to have at least one trash and recycling enclosure. All enclosures shall be of adequate size and configuration to accommodate separate containers for the collection of waste and recyclable materials.

Typical enclosures shall utilize 6-foot high masonry walls with the exterior treatment matching that of the building. An overhead trellis shall be provided to support vines and to screen the view from above. Access gates of a solid wood or metal material shall also be provided. Vines or shrubs should be planted on three sides of an enclosure where possible. This will soften the appearance of the walls and help it to blend into the development.

### 3.6

#### **LOADING/EXTERIOR STORAGE**

Commercial and R&D developments will provide loading spaces in accordance with Section 19.46 of the City of Camarillo Zoning Ordinance. Loading areas must be

designed to provide for backing and maneuvering on site. Loading areas shall not encroach into setbacks and all loading areas must be screened from adjacent parcels and streets.

Outdoor storage will not be permitted, unless approved by the Director of Community Development with conditions for screening. Garden centers that are enclosed and designed to conform with the adjacent building may be allowed, subject to approval by the City.

#### 4.0

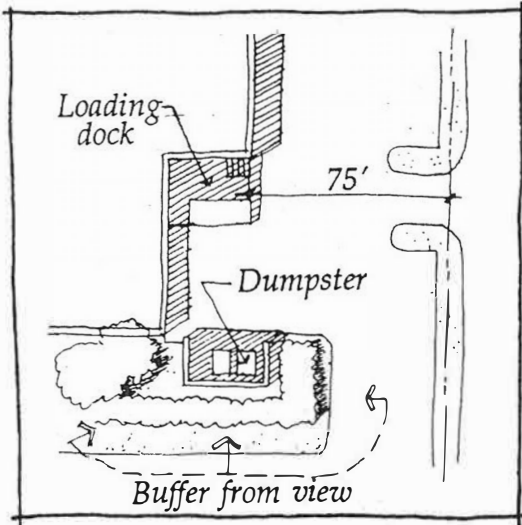
### **SIGNAGE**

A quality signing program will be administered by the Planning and Community Development Department. Besides the City of Camarillo sign requirements, the Specific Plan will apply further conditions as outlined below.

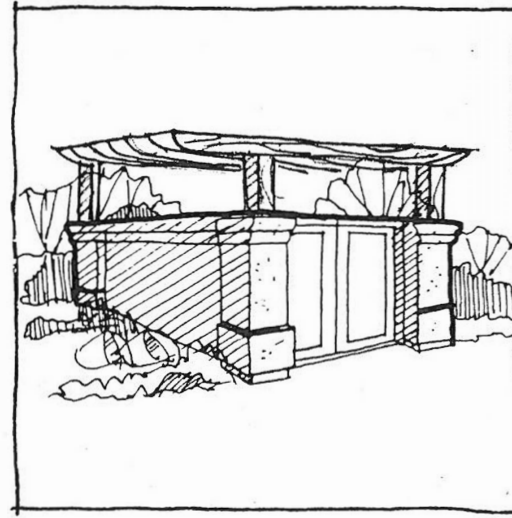
#### 4.1

### **PROJECT ENTRY MONUMENT**

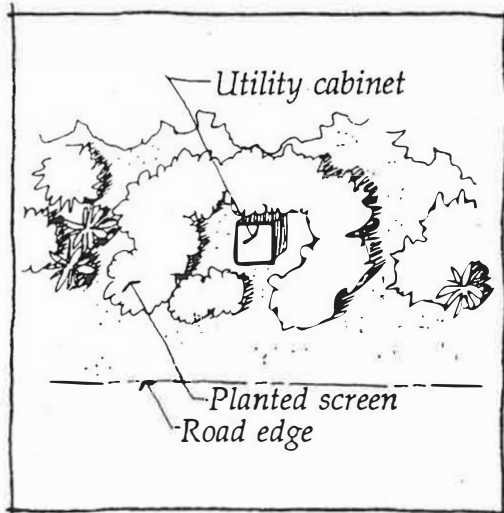
Two areas along Las Posas Road are designated as major entries and identification points for the Prado de Las Posas Specific Plan area. These major entries will be constructed and maintained by the developer.



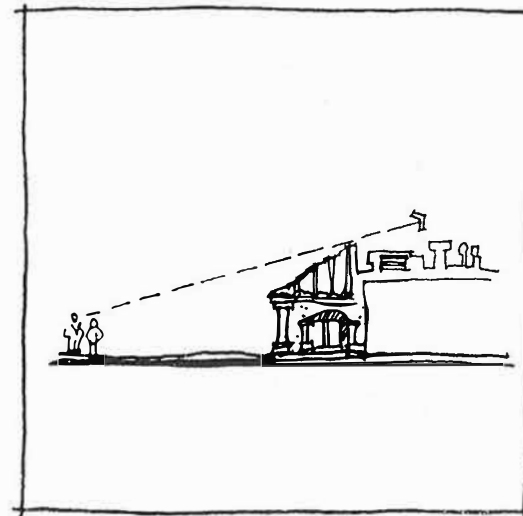
*Refuse and loading areas shall be located to the rear or side of buildings and screened*



*Refuse enclosures shall reflect the architecture of surrounding buildings*



*Utility cabinets shall be screened with plantings*



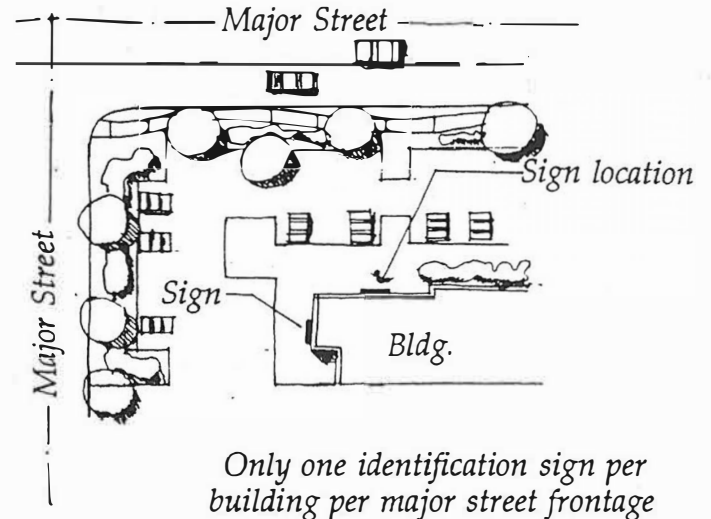
*Rooftop mechanical equipment shall not exceed the height of the parapet*

FIGURE 30 Screening





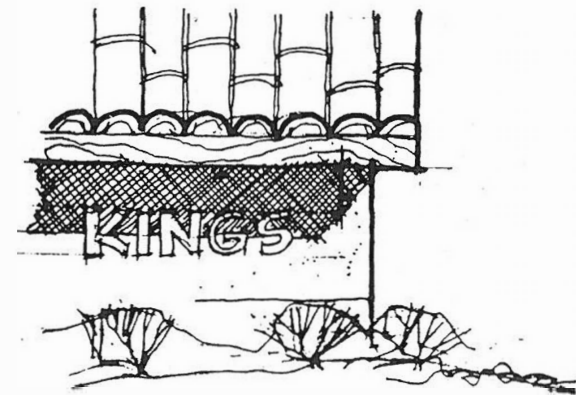
*Hanging or projecting signs below portales*



*Only one identification sign per building per major street frontage*



*Signs two feet below the top of the parapet*



*Wall signs below building eave*



The entry intersections with their monuments will depict the style of Prado de Las Posas. Decorative walls, incorporation of water features, and flowering plant materials with splashes of color will portray the Mediterranean flavor of Prado de Las Posas.

#### 4.2

##### **SECONDARY ENTRY MONUMENT**

The intersections at both ends of the private driveway at Ventura Boulevard and Camarillo Center Drive have been selected for smaller scale entry monuments. Both major and minor entries will be the developer's responsibility.

#### 4.3

##### **BUSINESS SIGNS**

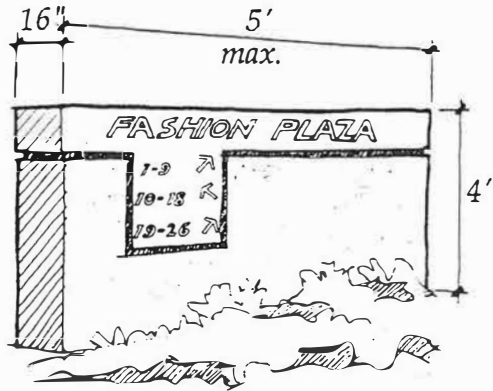
Business signs where authorized by the Sign Ordinance would consist of pole signs, monument signs and wall-mounted signs. Pole signs are permitted under certain specific instances as specified by the Sign Ordinance either to advertise the name of the shopping center or the name of the center plus up to four uses within the commercial location. The pole supporting the sign will be required to be boxed in and finished with a stucco to match the main building. The face of the sign would be limited to earthtone colors with key accent colors for lettering which would follow a corporate design program. Pole signs may be internally illuminated.

Monument signs may be permitted for individual businesses in the research and development area or for individual businesses on separate parcels if not part of the center. The monument signs would be not more than 8 feet in height and constructed of either concrete or masonry. The face of the sign may be internally illuminated. Again, the background of the sign should be of earthtone colors. Accent colors may be those which follow the colors used for the corporate identification. For businesses which are not permitted either a pole sign or monument sign, a wall-mounted sign may be authorized by the Sign Ordinance. The size, shape and materials used for the wall-mounted sign will follow a master sign program which will be established for uses within a center.

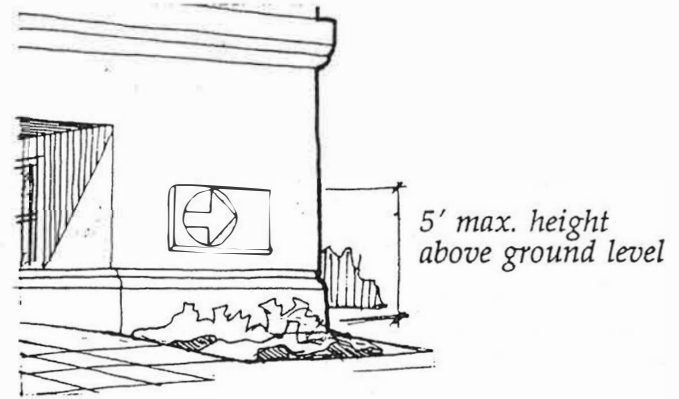
#### 4.5

##### **PUBLIC CONVENIENCE SIGNS**

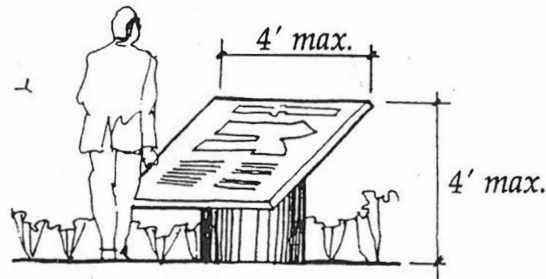
Public convenience signs under certain instances are permitted under the Sign Ordinance. These signs would include entry and exit directions, shipping and receiving signs, and other similar type signs which would direct the flow of traffic. The public convenience signs would be required to be reviewed as part of the master sign program for the development and would be required to be coordinated with the main business signs.



*Locates businesses within an individual building complex.*



*Wall mounted or integrated into the building.*



*Pedestrian directional monuments locate establishments within an individual complex.*



## 5.0

### LANDSCAPE

#### 5.1

##### PURPOSE AND CONCEPT

The landscape theme will play a significant role in establishing the character for Prado de Las Posas. This section is designed to organize development of the park area and integrate the hardscapes and perimeter treatments in a visually pleasing manner. Through the use of appropriate landscaping elements, coupled with Spanish or Mediterranean architecture, an attractive landscape program can be established.

#### 5.2

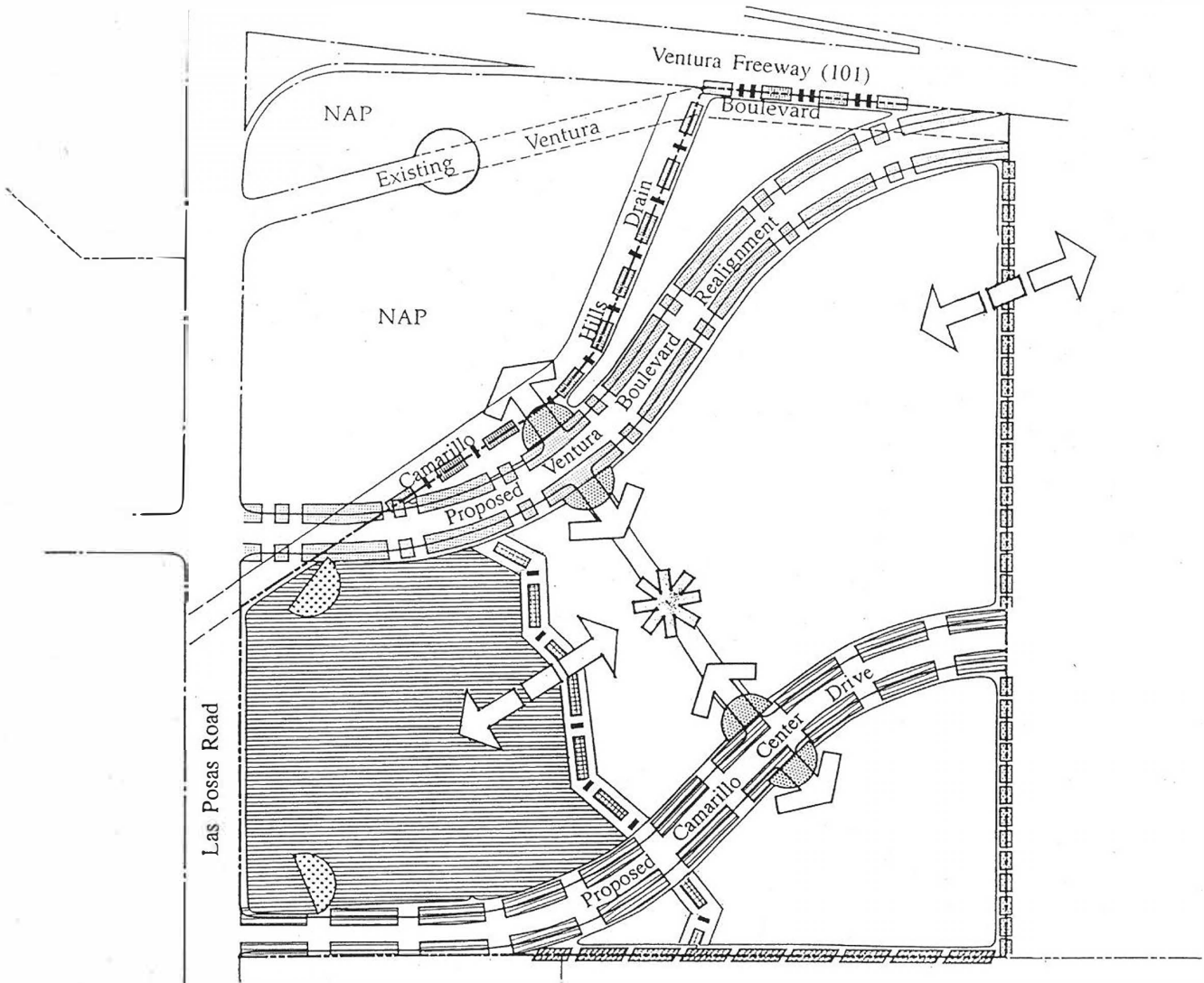
##### STREETSCAPES AND EDGE TREATMENTS

There are two levels of streetscape treatment within Prado de Las Posas.

Ventura Boulevard and Camarillo Center Drive are major corridors through the project that set the overall development theme. Landscape treatment will be consistent with the Koll Leonard Specific Plan to the east. A 30-foot easement outside the right-of-way on both sides of each road will provide a generous area for landscaping.



Landscaping along Ventura Boulevard shall have undulating, mounds with a maximum height of 3½ feet within the parkway and setback area. Trees shall be planted at a density of not less than 6 per 100 lineal feet with clusters no further than 100 feet apart.






KEY

-  STREETScape #1
-  STREETScape #2

-  PROJECT ENTRY
-  SECONDARY ENTRY

-  PEDESTRIAN CONNECTION

-  AUTO CORE

-  PARK/OUTDOOR RECREATION/  
PARKING/RETENTION BASIN

-  EDGE TREATMENT #1
-  EDGE TREATMENT #2
-  EDGE TREATMENT #3
-  EDGE TREATMENT #4
-  EDGE TREATMENT #5

LANDSCAPE SETBACK      BUILDING SETBACK

0' OR 10'	46' OR 20'
20'	71' OR 76'
0'	0'
15'	30'
5'	5'

SCALE: 1"=400'

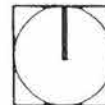


FIGURE 33 Conceptual Landscape Plan



Informal massing of trees with contrasting open space is encouraged. Forty percent (40%) of the trees shall be 24" box size or greater and sixty percent (60%) of the trees shall be 15 gallon size or greater. These landscaped areas must have an understory of shrubs and turf. Turf areas shall not exceed 10% of the total landscaped area. To soften building elevations, shrubs shall be provided where buildings are adjacent to streets. Landscape areas outside the curb shall be part of the common area.

A median planted with informal clusters of trees, shrubs, and groundcover shall be installed in Ventura Boulevard. Similar to the street edge, tree spacing shall average 6 per 100 feet with clusters no farther than 100 feet part. The same mixture of tree sizes shall apply.

#### **Street Trees for Ventura Boulevard:**

##### Parkway Street Tree

London Plane Tree      *Platanus acerifolia*

##### Parkway Secondary Tree

Canary Island Pine      *Pinus canariensis*

##### Median Tree

Canary Island Pine      *Pinus canariensis*

##### Median Accent Tree

Coral Tree      *Erythrina caffra*

Landscaping along Camarillo Center Drive shall consist of a generous 40-foot greenspace (10-foot public right-of-way plus 30-foot setback) landscaped area. An undulated berm with a maximum height of 3½ feet in the parkway setback area will be required.



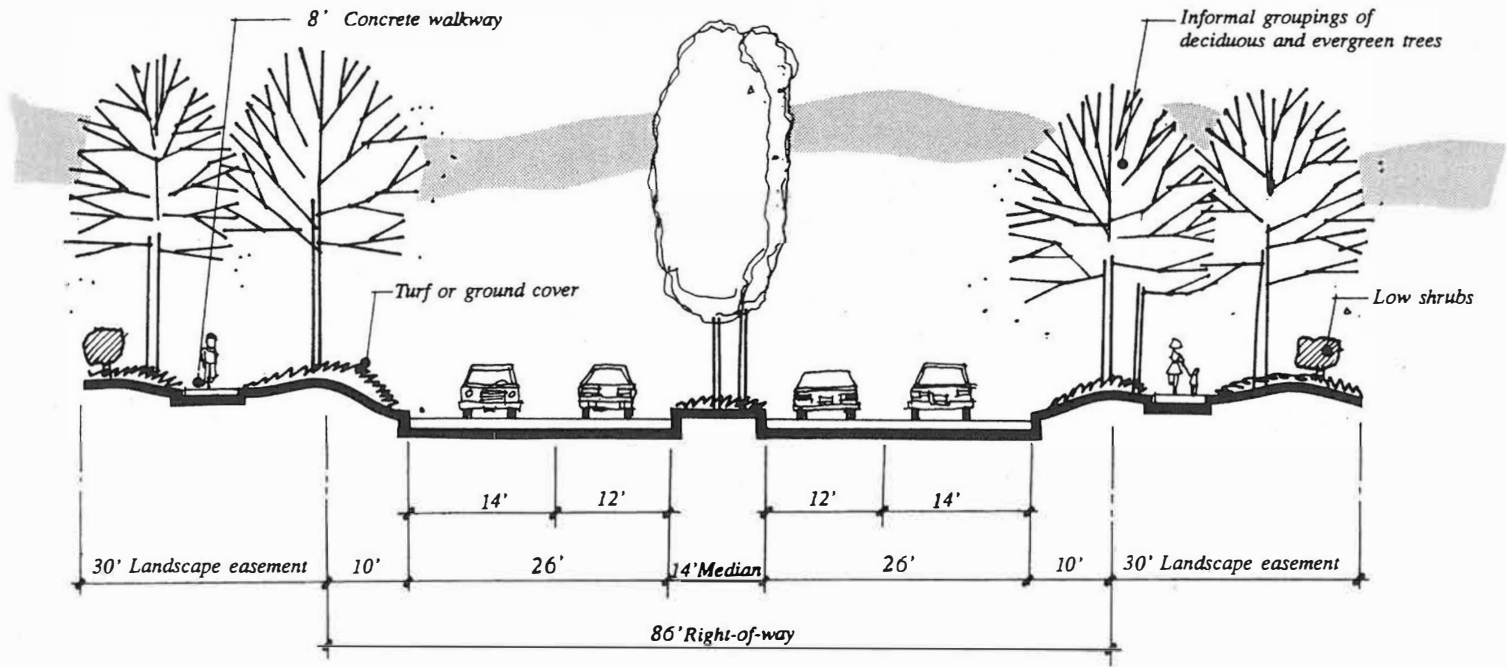


FIGURE 34 Ventura Boulevard Streetscape #1



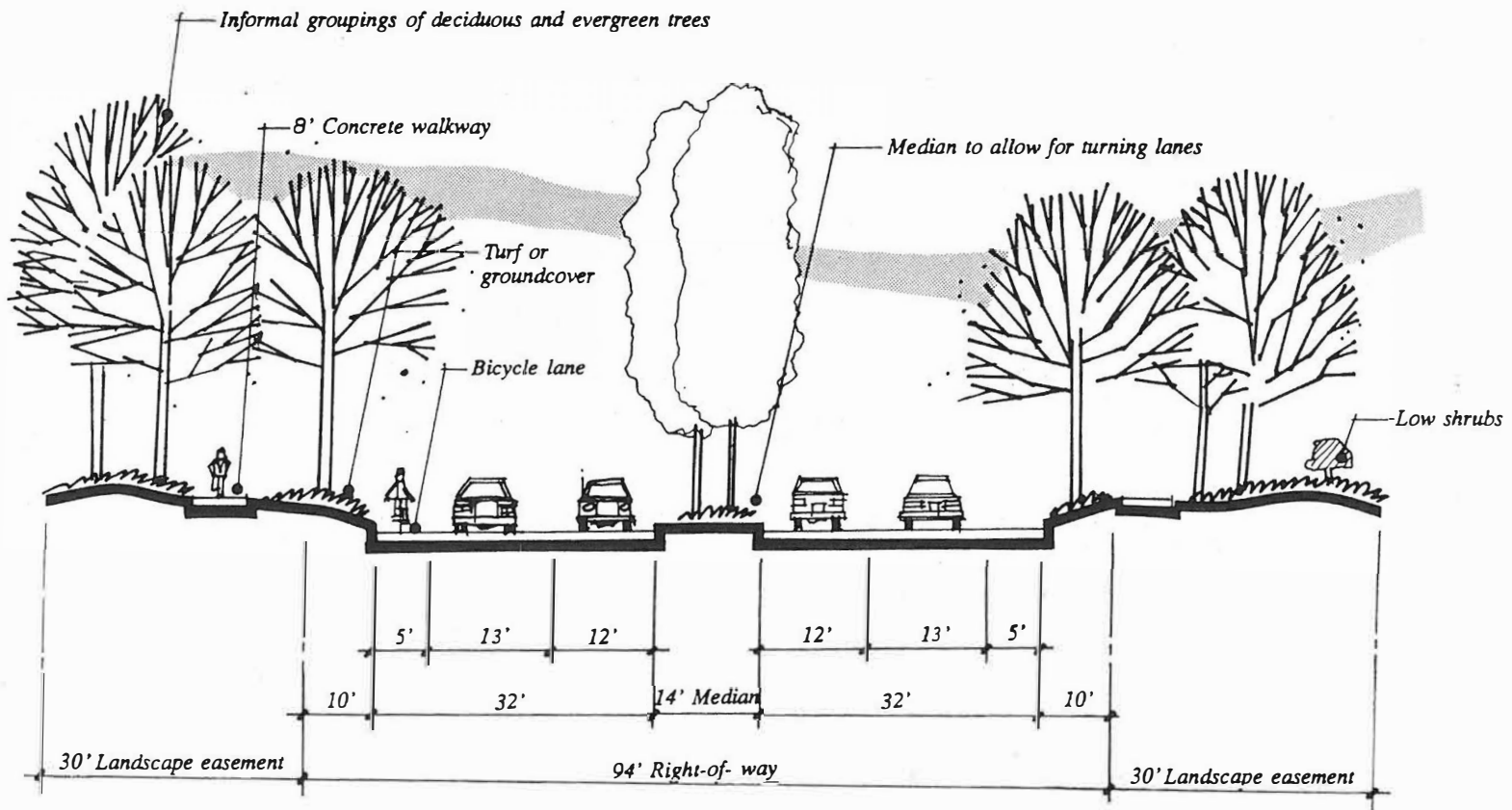


FIGURE 35 Camarillo Center Drive Streetscape #2



Trees shall be planted with a density of 6 per 100 feet, with clusters no farther than 100 feet apart. Forty percent (40%) of the trees shall be 24" box size or greater and sixty percent (60%) of the trees shall be 15 gallon size or greater. These landscaped areas must have an understory of shrubs and turf. The maximum turf coverage shall not exceed ten percent (10%). To soften building elevations, shrubs shall be provided where buildings are adjacent to streets. Landscape areas outside the curb are a part of the common area.

A median planted with informal clusters of trees, shrubs, groundcover and colorful annual flowers shall be installed in Camarillo Center Drive. Similar to the parkway setback landscape area, tree spacing must average 6 per 100 feet with clusters no farther than 150 feet apart. The same mixture of tree sizes shall also apply.

#### **Street Trees for Camarillo Center Drive:**

##### Parkway Street Tree

Southern Magnolia      *Magnolia grandiflora*

##### Parkway Secondary Tree

Tulip Tree      *Liriodendron tulipifera*

#### **Street Trees for Secondary Interior streets:**

##### Parkway Street Tree

White Alder      *Alnus Rhombifolia*

##### Parkway Secondary Tree

Brisbane Box      *Tristania Conferta*

An 8-foot wide meandering sidewalk shall be installed on both sides of the street. For safety, curving segments must permit an unobstructed view of 200 feet.

#### **Interior Driveways**

Landscaping along interior driveways must consist of low relief groundcover and shrubs. Linear segments of tall dense shrub is discouraged. The intent is to maintain the shared pedestrian/automobile environment.

Use of informal clusters of street trees at intersections and pergolas are encouraged. Forty percent (40%) of the trees shall be 24" box size or larger and sixty percent (60%) shall be 15 gallons or larger.

**Primary Interior Driveway** - (Segment between Ventura Boulevard and Camarillo Center Drive)

#### Parkway Tree

Jacaranda

*Mimosi folia*

#### Secondary Tree

Tulip Tree

*Liriodendron tulipifera*

**Secondary Interior Driveway Trees** - (adjacent to pedestrian spine and other driveways, entrances and parking lots)

#### Parkway Tree

White Alder

*Alnus Rhombifolia*

#### Secondary Tree

Brisbane Box

*Tristania conferta*

At grade, deciduous trees should be planted along the internal driveway, parking plaza and pedestrian spine edges. They should be surrounded by rolled curbs and iron grates. Islands of raised and at-grade planters with lush shrubs and ground covers may be used to mark focal points and entries.

Planting areas between walls and streets should be landscaped in a hierarchy of plants with informal groupings. Solid block walls 3 feet or higher should receive vines from the planting palette. Turf, low shrubs or low ground cover should be planted within the right-of-way.

Development plan landscaping should relate to adjacent street plantings.

### 5.3

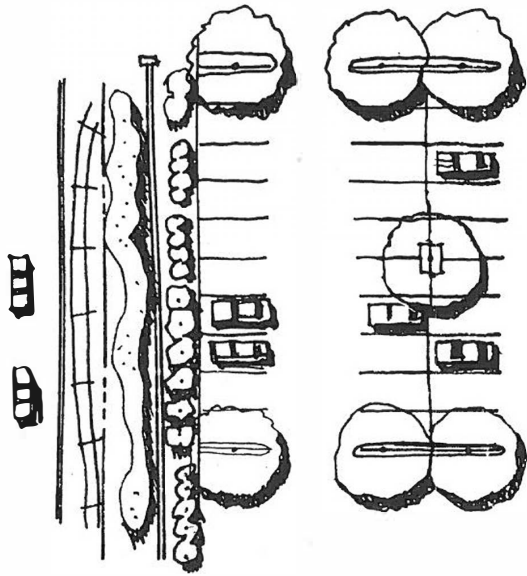
#### PARKING AREAS

Parking areas should be surfaced with asphalt and striped to mark parking stalls. Medium size shade trees should be planted at a coverage of one tree per ten parking stalls on single-loaded parking and five trees per twenty stalls on double-loaded parking (refer to Figure 36). Direct views into parking areas from Camarillo Center Drive or Ventura Boulevard should be screened by low walls, berms, hedges or shrubs (3 feet maximum).

#### Parking Lot Trees

London Plane Tree	<i>Platanus acerifolia</i>
Evergreen Pear	<i>Pyrus kawakani</i>
Queen Palm	<i>Arecastrum romanzoffianum</i>





Medium size shade trees in fingers or cement boxes should be planted at a coverage of one tree per ten parking stalls on single-loaded parking and five trees per twenty stalls on double-loaded parking. 6' X 9' box for every 10 double-loaded parking spaces.

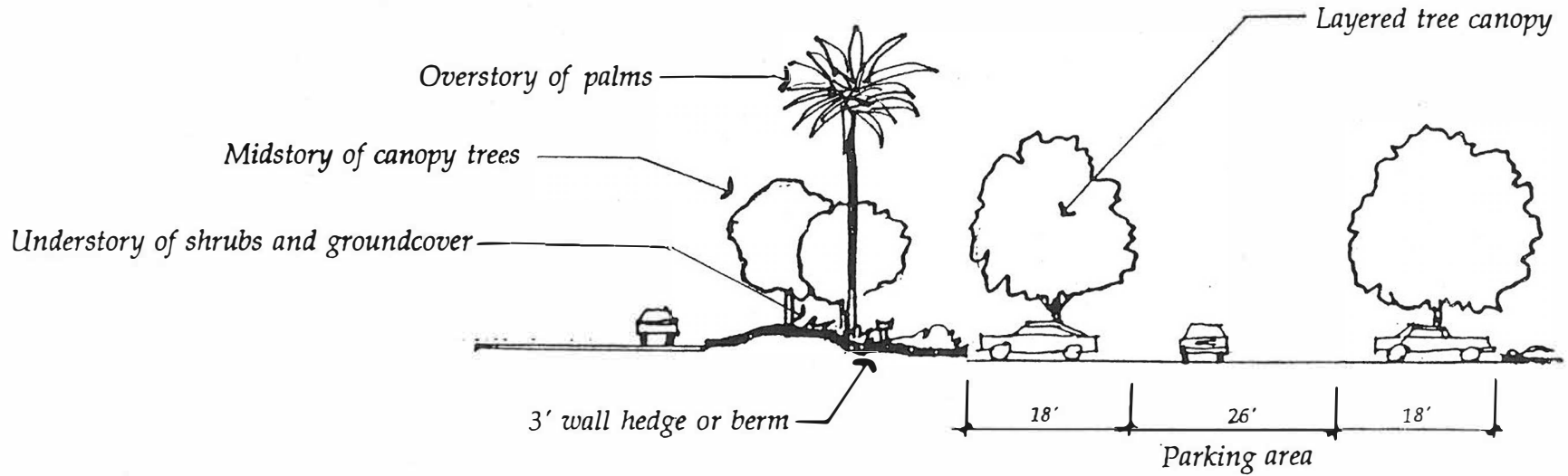


FIGURE 36 Parking Area Landscape



#### 5.4

##### SCREENING

Landscape treatment along the northern property boundary and adjacent to Ventura Boulevard should be designed to permit visual penetration from Ventura Freeway (US 101). A 20-foot landscape treatment along the southern property boundary should screen views of agricultural activities, utilizing a palette consistent with the Koll Leonard Specific Plan. A 10-foot landscape treatment along the easterly property line between Camarillo Center Drive and the southeast corner of the site shall also use a palette consistent with the Koll Leonard Specific Plan. These treatments include dense plantings of Leland Cypress (*Cupressocyparis leylandii*) and American Sweetgum (*Liquidambar styraciflua*).

Landscape treatment along the east property boundary should screen service areas from the view of adjoining properties, except where a common service corridor is used. Landscaping of the service areas or service corridor shall be accomplished using the building plant palette.

Screening may be required for privacy or aesthetic purposes in a development. The type of screens should be analyzed and the opportunity to create a partial screen by using perforated walls is encouraged. Screening should be done with plant massing, trees and/or mounding. All screening will be reviewed on a case-by-case basis to ensure cohesiveness throughout Prado de Las Posas.



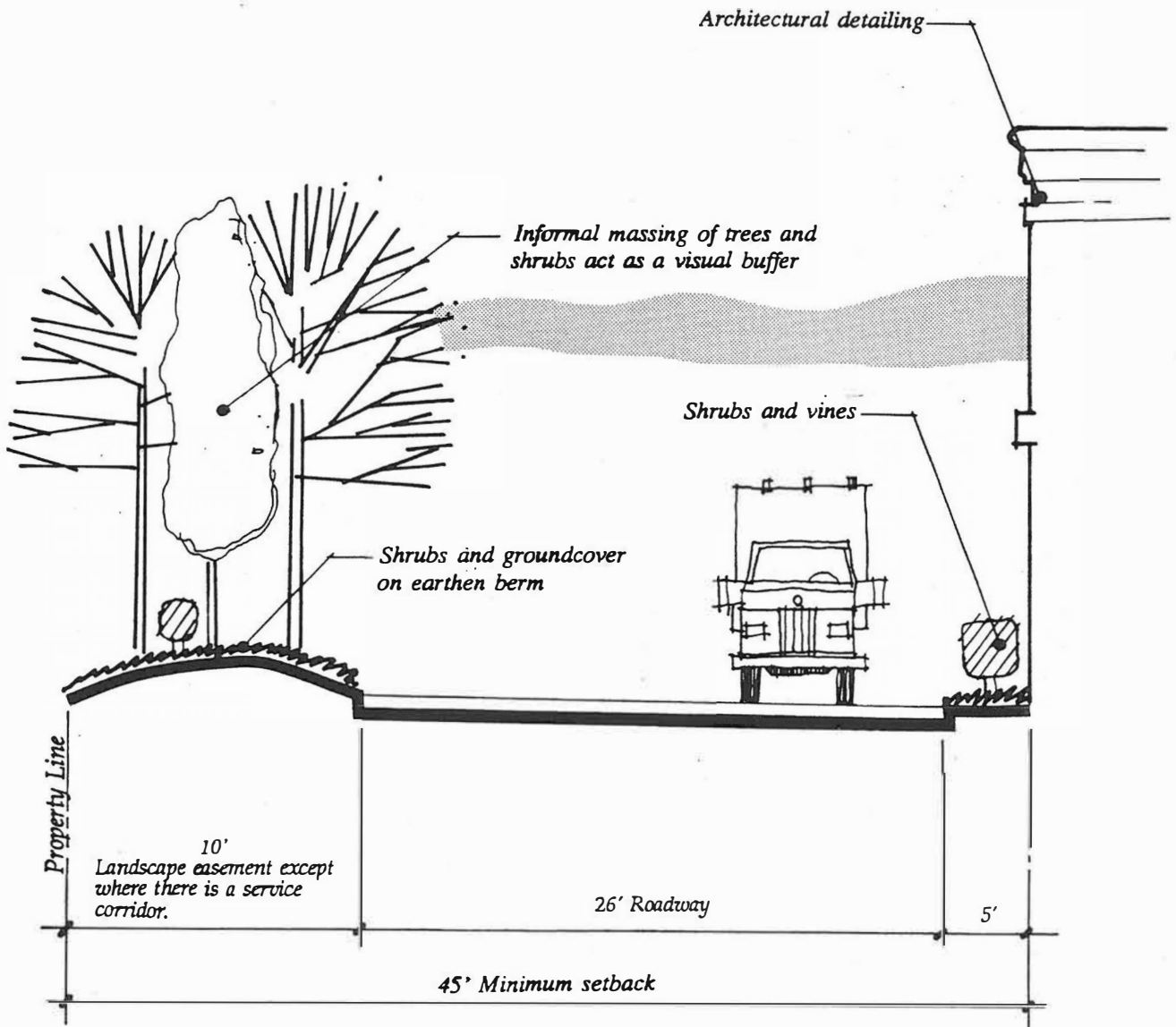


FIGURE 37 Edge Treatment #1



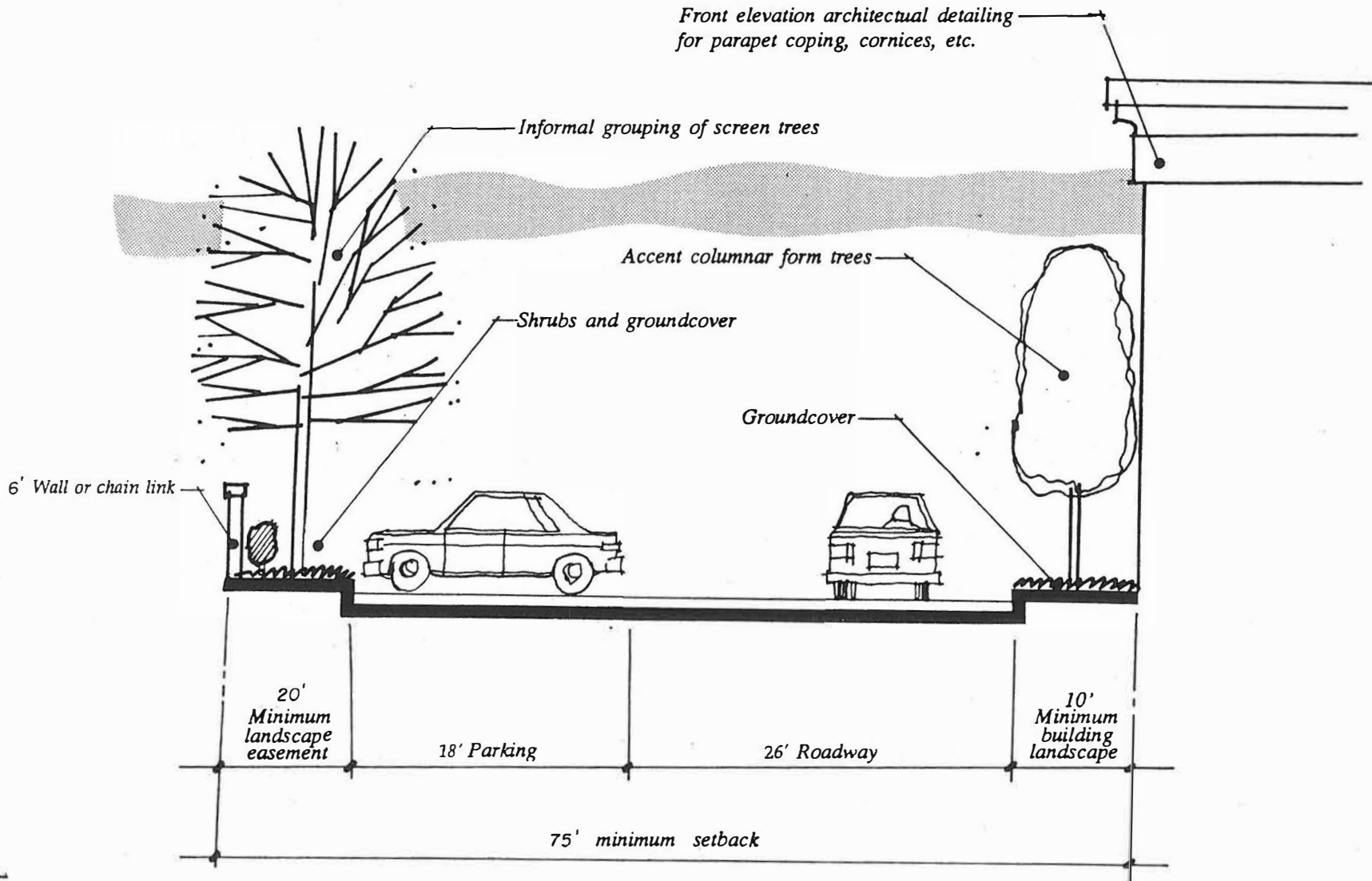


FIGURE 38 Edge Treatment #2



Partial screens may be appropriate adjacent to the Camarillo Hills Drain. Landscaping adjacent to the Camarillo Hills Drain shall be provided with combinations of wall or fence and plant materials encouraged. View fence made of wrought iron is acceptable. Where solid walls are used, vines or shrubs are required to soften their appearance.

### 5.5

#### **PARK/OUTDOOR RECREATION/RETENTION AREA**

Landscape materials selected for the park/outdoor recreation/retention area should consider Camarillo Airport to the west. Landscape material should not penetrate the avigational height limit. This may be achieved by pruning or use of plant material with a mature height lower than the avigational limits. Solid, rigid plant materials are to be avoided. Trees may be selected from the following list:

#### Special Treatment Area Trees

London Plane Tree	<i>Platanus acerifolia</i>
White Alder	<i>Alnus Rhombifolia</i>
Canary Island Pine	<i>Pinus canariensis</i>
Coral Tree	<i>Erthrina caffra</i>
Carrotwood	<i>Cupaniopsis anacardioides</i>
Purple Leaf Plum	<i>Prunus blireiana</i>



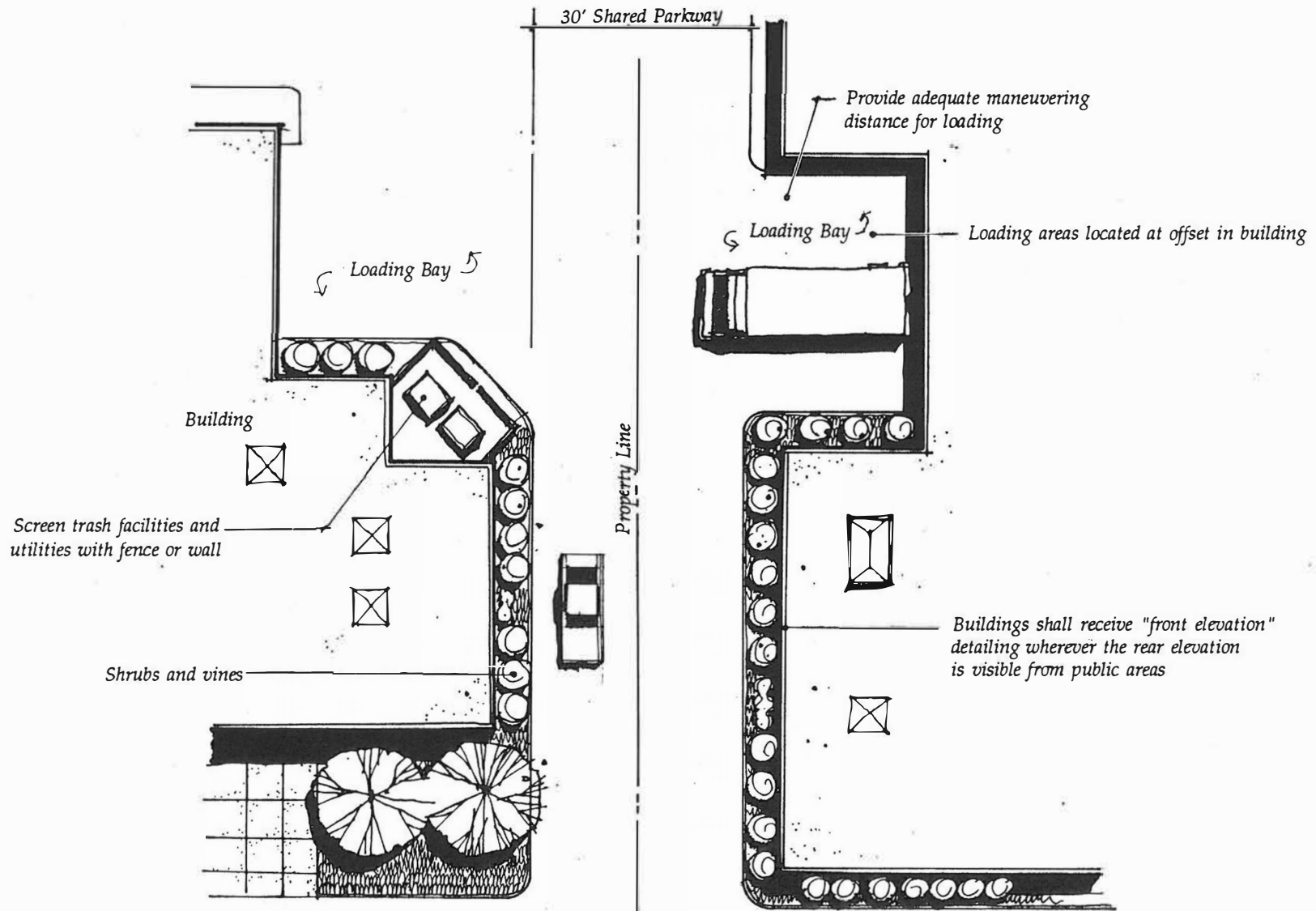


FIGURE 39 Service Corridor



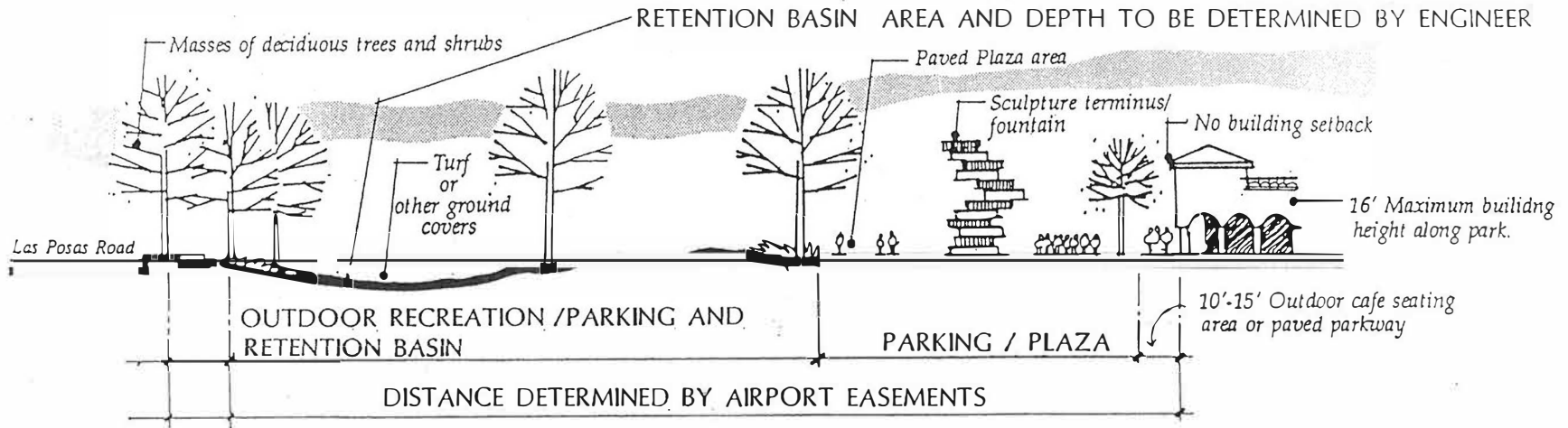


FIGURE 40 Edge Treatment #3 Park/Outdoor Recreation/Retention



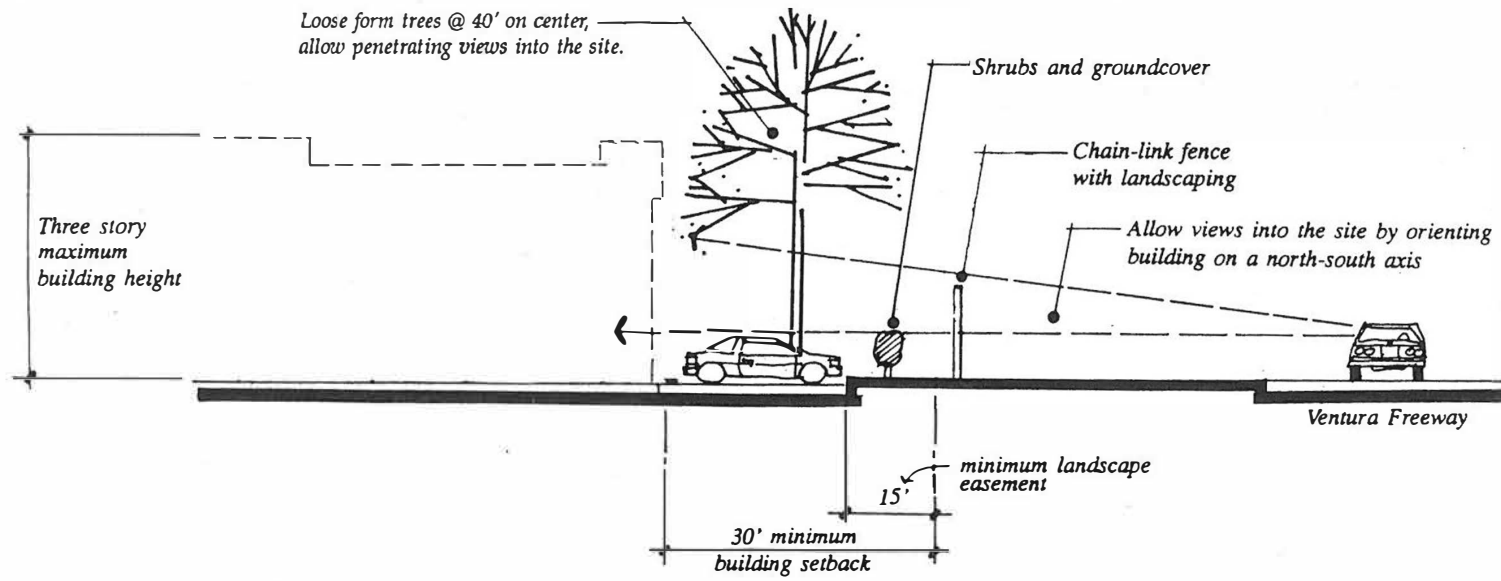


FIGURE 41 Edge Treatment #4



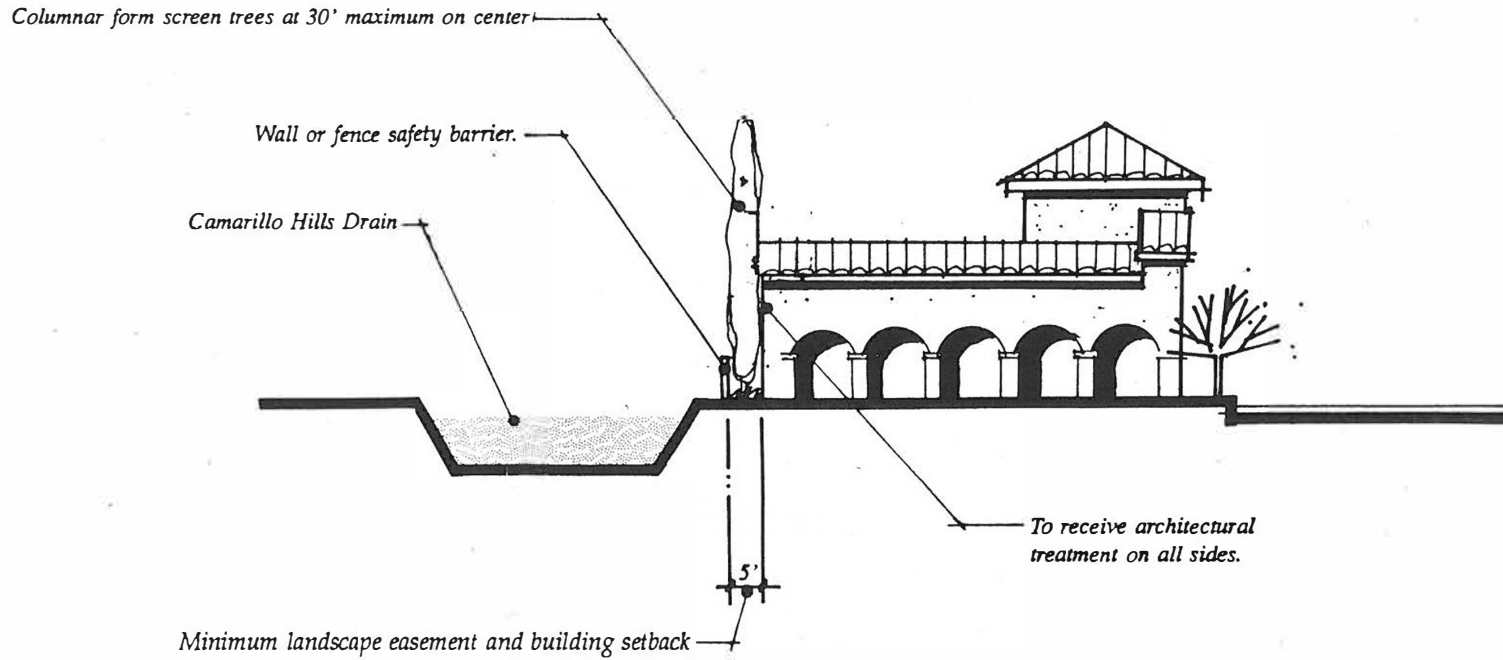


FIGURE 42 Edge Treatment #5



**5.6****ENTRIES**

Project entries shall be located at the northwest and southwest corners of the outdoor recreation retention area (Planning Area III), adjacent to Ventura Boulevard and Camarillo Center Drive, respectively.

The entry should be densely planted with multiple rows of accent trees, shrubs and turf. This planted area shall be framed with a stucco masonry or concrete wall.

Secondary entries shall be located at the two main intersections between the private driveway and Ventura Boulevard and Camarillo Center Drive. These secondary entries shall be set back a minimum of 125 feet from the centerline of Ventura Boulevard or Camarillo Center Drive and the centerline of the private driveway. See Figure 44.

Secondary entries shall be landscaped with a single row of regularly spaced accent trees and backed up with a low evergreen hedge and a low stucco masonry wall. Turf, colorful annuals and an entry monument may be located in the foreground.



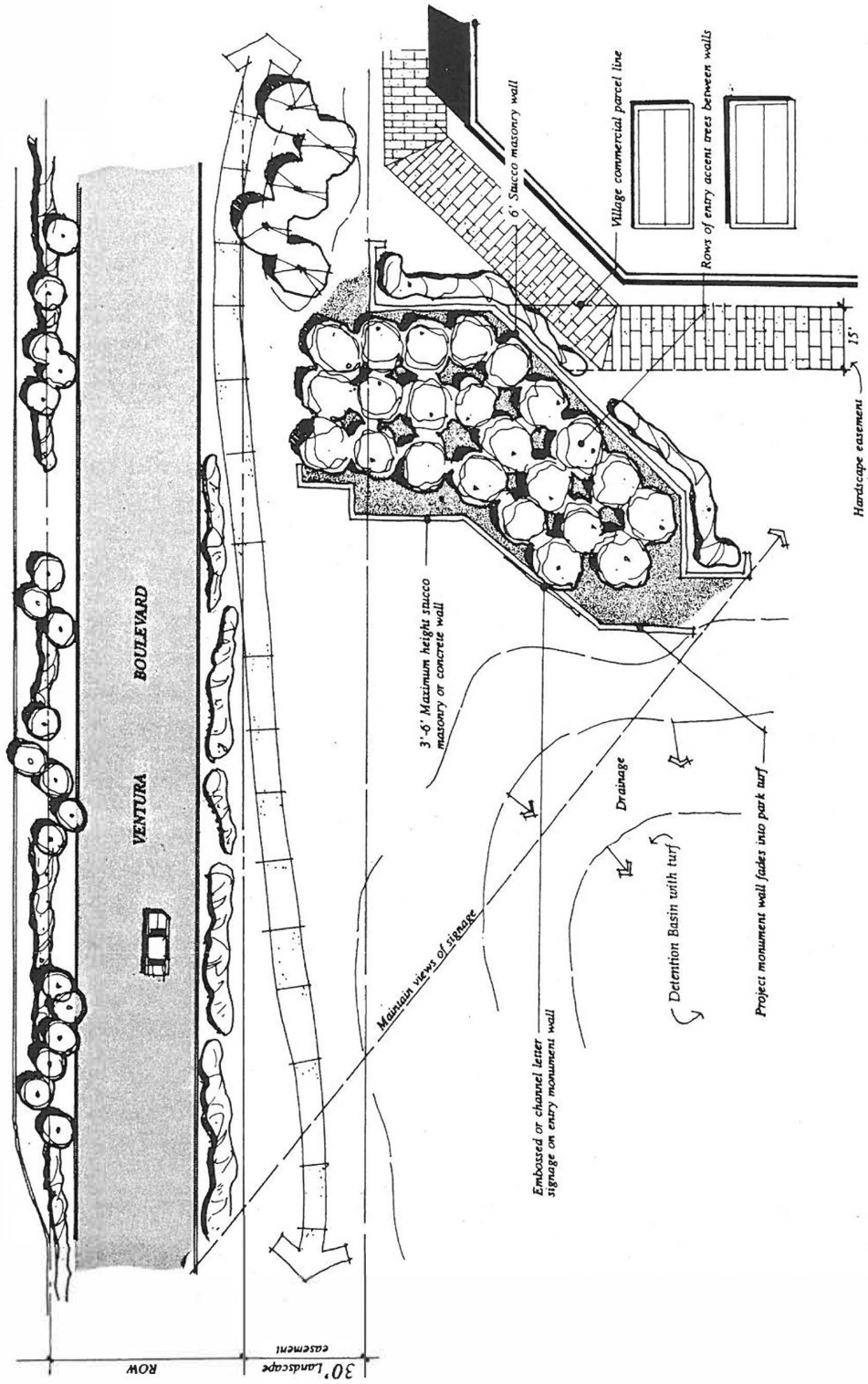


FIGURE 43 Project Entry



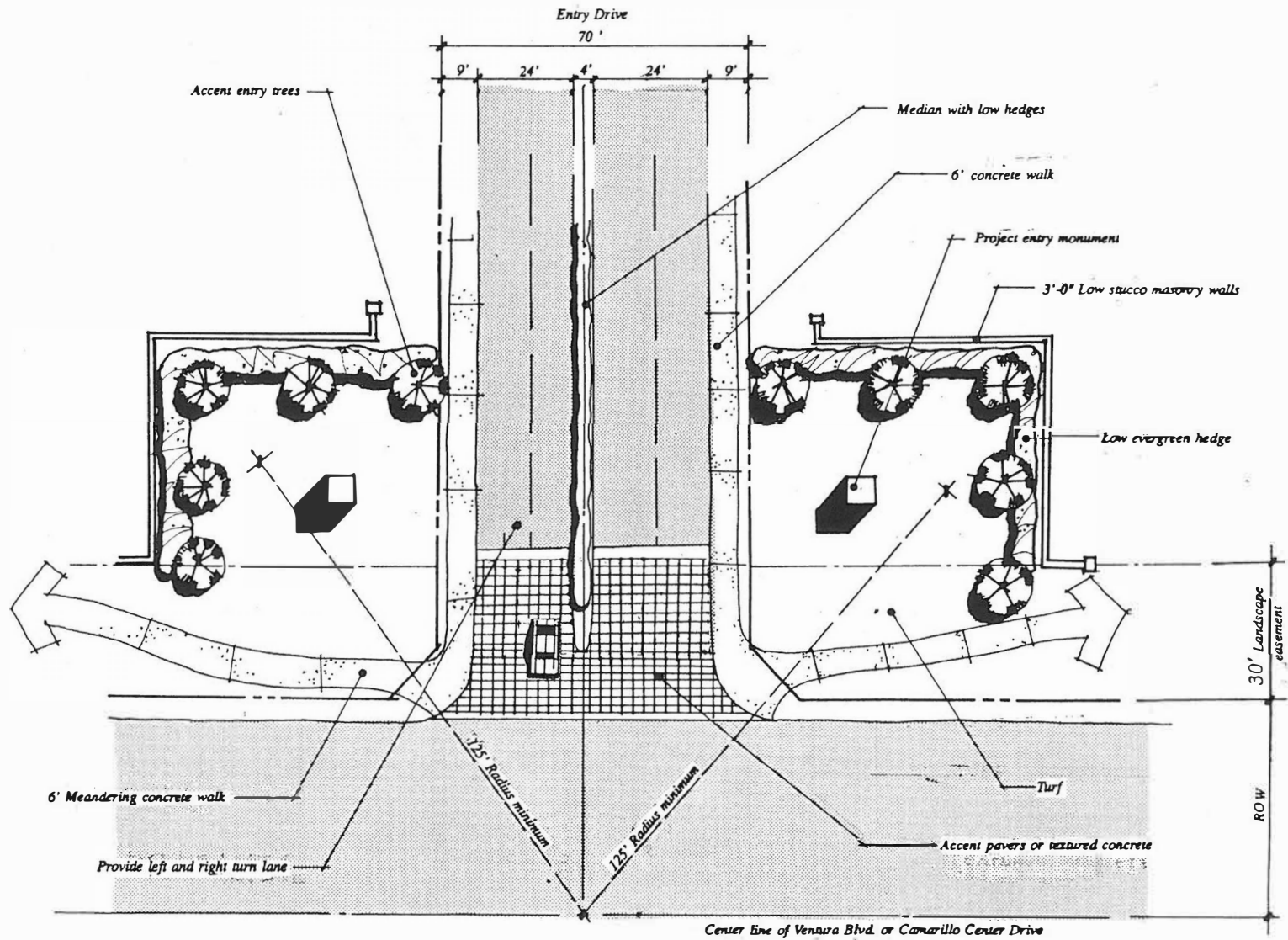


FIGURE 44 Secondary Entry



<b>Introduction</b>	<b>• 1.0</b>
<b>Processing, Procedures and Phasing</b>	<b>• 2.0</b>
<b>Comprehensive Maintenance</b>	<b>• 3.0</b>
<b>Camarillo General Plan Consistency</b>	<b>• 4.0</b>



## 1.0

### INTRODUCTION

Implementation of the Prado de Las Posas Specific Plan involves five distinct elements: General Plan consistency, entitlements, construction, leasing and maintenance. Together, these elements work to guide and ensure faithful development of the Plan.

The first function of any Specific Plan is to implement the community General Plan. Adoption of this Specific Plan will implement all the policies and standards contained within the document as part of the General Plan.

Also identified in this section are plan processing steps, including the general plan amendment and zone change procedures that will be required. In addition, procedures for Specific Plan amendments, planned development permits, subdivisions, development plans and development agreements are outlined.

The Comprehensive Maintenance section describes maintenance responsibilities of the land owner, project tenants and the City.

As noted in the EIR and Development Plan sections, constraints exist in the form of height limitations drainage, traffic and loss of agricultural lands. The proposed development will use the Specific Plan process to ensure that the development plan, development standards and design guidelines are followed. In addition, the Specific

Plan recognizes the EIR mitigation measures and incorporates those measures into the development plan, development standards and design guidelines. The Specific Plan presents controls which will work with the Zoning Ordinance to create a vibrant commercial-retail development with a cohesive theme.

## 2.0

### **PROCESSING, PROCEDURES AND PHASING**

Public hearings before the Planning Commission and City Council represent the first official step towards implementing the proposed Specific Plan. Plan adoption, by resolution, will set the tone for development. The design guidelines, including land use relationships, street alignments, building design and landscaping requirements will provide the developer with a direction to proceed in formulating precise development plans for the various land uses.

## 2.1

### **AMENDING THE SPECIFIC PLAN**

#### **Requirements and Procedures**

This plan may be amended in the same manner and by the same process by which the plan was originally adopted. An environmental information form shall be prepared, but the presumption is that, only if intensification of development beyond the adopted Specific Plan threshold were to be approved, a supplemental EIR would be required.

**Administrative Actions**

Certain changes to explicit provisions in the Specific Plan may be made administratively by the Planning and Community Development Director or Director of Engineering Services subject to appeal to the Planning Commission; and, subsequently to the City Council if required. These changes include:

- Additional information that does not change the effect of any concepts or regulations.
- Changes to the community infrastructure, such as drainage, water, and sewer systems which do not have the effect of increasing or decreasing development capacity in the Specific Plan area, nor change the Plan concepts.

**2.2****ZONING**

While the Specific Plan sets forth various land uses for the total project, zoning for those uses will not occur until a precise plan has been submitted for review and approval. Upon approval of the precise plan, the applicant may file a zone change application to establish precise zone boundary lines within the Specific Plan area. This action may occur concurrent with processing of the Planned Development Permits.

### 2.3

#### **PLANNED DEVELOPMENT PERMITS**

Prior to approval of building permits, the various zone categories (CPD and L-M) require Planning Commission approval of Planned Development Permits. The Planned Development Permits will show precise building configurations, parking lot designs and exterior building elevations. Conditions of development will set forth required improvements for streets, drainage, utilities and other development requirements.

### 2.4

#### **PHASING**

Phasing of the Specific Plan will be identified at the time of planned development permits or submittals. The phasing plan will indicate what areas will be developed initially and those areas that will be developed in subsequent phases. The first phase will include roadway improvements required for Ventura Boulevard in addition to back bone infrastructure (water, sewage, drainage, gas, electric, telephone and cable) improvements within the street right-of-way.

Ventura Boulevard improvements may require the installation of a bus turn-out including landscape and passenger facilities. The internal street system, pedestrian linkage and infrastructure will be constructed as required in order to support other phases of development.

Development of the first planning area contiguous to Camarillo Center Drive shall include street and infrastructure improvements.

Landscaping, hardscape and the retention basin in Planning Area III shall be completed when Ventura Boulevard or Camarillo Center Drive is built to full width.

## 2.5

### **PERFORMANCE STANDARDS**

R&D development will be subject to the City of Camarillo Zoning Ordinance performance standards. These performance standards address maximum levels for noise, smoke, odor, dust, vibration and other dangerous or objectionable elements which might otherwise adversely affect the surrounding area.

## 2.6

### **SUBDIVISION ORDINANCE**

Compliance with the Subdivision Ordinance is required where properties are to be separately financed, sold, leased or otherwise conveyed. The subdivision map will allow for phasing of development and the creation of parcels for various uses within the area. Further division of parcels for condominium offices will also be subject to provisions contained in the Subdivision Ordinance. The approval of subdivision maps, as with the approval of Planned Development Permits, will set forth various conditions of approval necessary to ensure street improvements, utilities, drainage features and other City requirements.

## 2.7

### DEVELOPMENT PLANS

The process to secure development permits will include submittal of grading/improvement plans, landscape plans, and building plans through the Engineering Services Department, Planning Department and Building and Safety Department, respectively. Checking of these plans will include reviewing the construction drawings to ensure that they conform to requirements of the Specific Plan, Planned Development Permits, Building Code, Grading Code and other City requirements prior to construction.

## 2.8

### SPECIFIC PLAN MODIFICATIONS

Minor modifications to the approved Prado de Las Posas Specific Plan may be allowed at the discretion of the Planning and Community Development Director. Such modifications may be considered during processing of the Development Plan Permit. Any modifications must be consistent with the purpose and intent of the originally approved Specific Plan document. All modifications or amendments to the approved Specific Plan, other than such minor changes, shall be processed as a Specific Plan Amendment and are subject to all specific plan procedures.

The maximum of 451,916 square feet of building space shall not be exceeded without a Specific Plan amendment. Uses permitted in the Development Yield Table represents the most intense development of the site. Less intense uses may be selected at the development plan stage, provided their impacts are equal to or less than those in the Development Yield Table.

### 3.0

#### COMPREHENSIVE MAINTENANCE

It is important to ensure that proper maintenance will be provided once the Planning Areas have been developed. The conditions of approval for various Planned Development Permits will require that individual sites are properly maintained.

The land owner or tenant association is responsible for maintenance of any bus turn-out, buffer areas and retention basin landscape and hardscape. In addition, landscape areas adjacent to the curb of: Ventura Boulevard, Camarillo Center Drive and Las Posas Road shall be privately maintained as common areas.

Maintenance of parking lots, sidewalks and common landscape areas within the property shall be the landowners' responsibility. A Declaration of Covenants, Conditions and Restrictions (CC&R's) may be required. If so, the CC&R's will provide for maintenance of common areas and also reciprocal access and parking easements among the parcels. In addition, the CC&R's will apply to maintenance of common parking, access, signage, landscaping and other common facilities.

#### **Public Roads**

The City of Camarillo will be responsible for the maintenance of public roads including landscaped medians and utilities within the right-of-way. Specifically, these public roads include: Ventura Boulevard, Camarillo Center Drive and Las Posas Road.

Initial street improvement costs shall be borne by the developer as dictated by the phasing program.

### **Street Improvements**

Roadway and infrastructure improvements will be considered relative to the final phasing plan proposed. Some progress towards the improvement of Ventura Boulevard will be required with the first phase of development. This could include dedication of the right-of-way, partial street improvements or other improvements as required. Should development of Planning Areas I and II occur with the first phase of development, improvement of Ventura Boulevard will be required at that time. Construction in the first planning area contiguous with Camarillo Center Drive may require extension of that street to Las Posas Road. Roadway width for the initial phase will be established by anticipated traffic volumes. Improvements will include landscaping of the parkways.

## **4.0**

### **CAMARILLO GENERAL PLAN CONSISTENCY**

The first step in implementing this specific plan will be the adoption of a General Plan amendment that incorporates by resolution the Prado de Las Posas Specific Plan into the Camarillo General Plan. The adoption of such a resolution would serve to implement all the policies and standards of this document as part of the General Plan. Developers, public agencies, and the City as a whole will then have a precise set of development standards and guidelines that will guide the preparation and implementation of precise

development proposals. Mandatory findings of consistency with the General Plan required for development proposals will also apply to the specific plan.

Specific amendments to the General Plan include:

- Change Land Use Element to remove the specific plan area from agriculture and apply the specific plan land use categories.
- Community Design Element to include the design standards (Chapter 5) for the specific plan area.

Chapters IV and V establish precise development standards for Prado de Las Posas. An application for a zone change may be filed at the same time as an application for development. Until that time, zoning in the area may remain as currently exists.

